

FED. ROAD DIST. NO.	STATE	U.S.P.W. PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WISC.	393-B	1	56

DIVISION JOB NO. 3501

**INDEX OF SHEETS**

SHEET NO	1	TITLE
SHEET NO	2	TYPICAL CROSS SECTIONS
SHEET NO	3	ESTIMATE OF QUANTITIES
SHEET NO	3A & 3B	DETAIL SUMMARY OF MISCL. QUANT. SPECIAL DETAILS.
SHEET NO	4 - 5	RIGHT OF WAY PLAT
SHEET NO	6 - 16	PLAN AND PROFILE STA. 0+00 TO STA. 288+58
SHEET NO	17-23	DRAINAGE STRUCTURES AND STANDARD DETAILS
SHEET NO	24-56	CROSS SECTIONS
SHEET NO		GRADE SHEET

**STATE OF WISCONSIN**  
**WISCONSIN HIGHWAY COMMISSION**

**PLAN AND PROFILE OF PROPOSED**  
**OSHKOSH ♦ FREMONT ♦ ROAD**  
**OSHKOSH - BUTTE DES MORTS**  
**WINNEBAGO COUNTY**

**N.R.H. - 393 B UNITED STATES PUBLIC WORKS PROJECT NO. N.R.H. - 393 B**

BEG. AT A POINT ON U.S.H. 110 NEAR THE INTERSECT. WITH MURDOCK ST. IN THE CITY OF OSHKOSH AT SURV. STA. 0+00 AND EXTEND'G. N-WESTERLY A DIST. OF 4.814 MILES TO A POINT ON U.S.H. 110 NEAR THE E. 1/4 COR. ON THE SO. LINE OF SECT. 19, T. 19N, R. 16E., AT SURVEY STA. 288+58.

R. 15 E. R. 16 E. SCALES PLAN 1 IN. = 100 FT. PROFILE HOR. 1 IN. = 100 FT. VERT. 1 IN. = 10 FT. CROSS SECTIONS HOR. 1 IN. = 5 FT. VERT. 1 IN. = 5 FT.

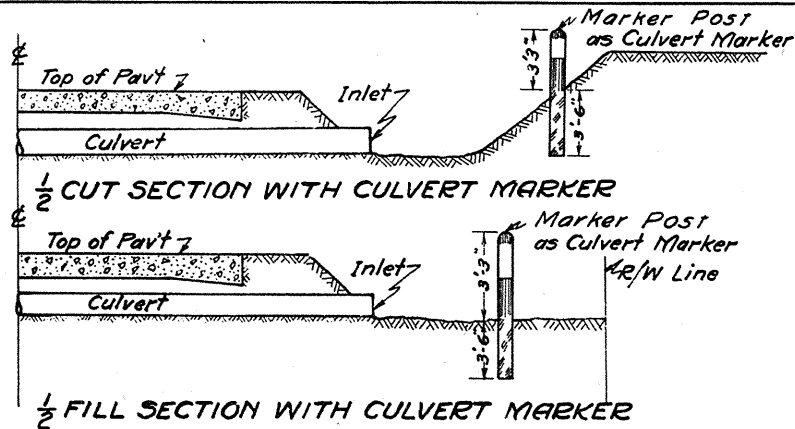


**CONVENTIONAL SIGNS**

STATE AND NATIONAL LINE.....	CULVERTS.....
COUNTY LINE.....	DROP INLET.....
CITY, VILLAGE, OR TOWN.....	TROLLEY POLE.....
TOWNSHIP LINE.....	POWER POLE.....
SECTION LINE.....	TELEPHONE OR TELEGRAPH POLES.....
FENCE LINE.....	MAIL BOX.....
GUARD RAIL.....	REFERENCE STAKES.....
UNFENCED PROPERTY.....	MARSH.....
RIGHT OF WAY LINE.....	HEDGE.....
TRAVELED WAY.....	TREES.....
RAILROADS.....	GROUND ELEVATION.....
RETAINING WALL.....	GRADE ELEVATION.....
LEVEE.....	
BASE OR SURVEY LINE.....	
R.O.W. MARKERS.....	

<b>WISCONSIN HIGHWAY COMMISSION</b> MADISON, WIS.		<b>U.S. DEPARTMENT OF AGRICULTURE</b> BUREAU OF PUBLIC ROADS	
Surveyor..... P.L.J.	Note Book 16084-7611A	RECOMMENDED FOR APPROVAL:	
Div. Computer A.J.S.	M. O. Checker.....	DISTRICT ENGINEER	
Div. Checker J.B.L.	Correct.....	CHIEF ENGINEER	
CORRECT: <i>N.A. Culbertson</i> DIVISION ENGINEER		APPROVED:	
RECOMMENDED FOR APPROVAL: <i>Harry D. Blake</i> <i>Frank Crane</i> ENGINEER - SURVEYS & PLANS SR. ASST. ENGR. - PLANS		DATE: 1/29/34	
APPROVED: <i>H.P. Parker</i> STATE HIGHWAY ENGINEER		DIRECTOR	

N.R.H. - 393 B

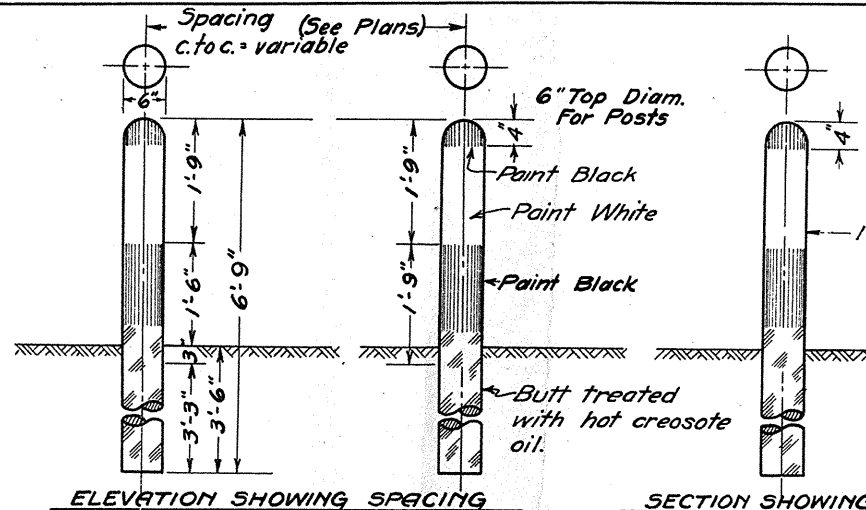


Marker Post as Culvert Marker to be placed at inlet end of culvert. Place post clear of channel.

In Cut, place post on back slope.

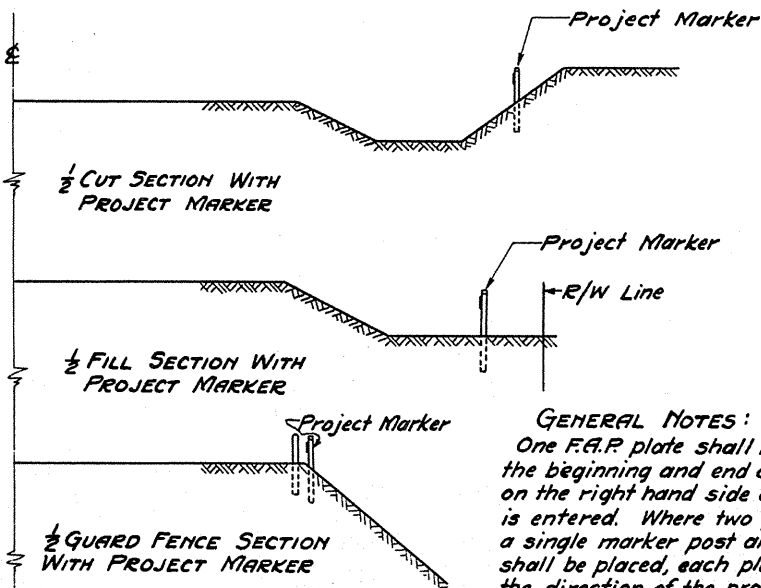
In Fill, place post beyond end of pipe so it is visible from the roadway.

Marker Posts for Right of Way Markers shall be placed where directed by the Engineer



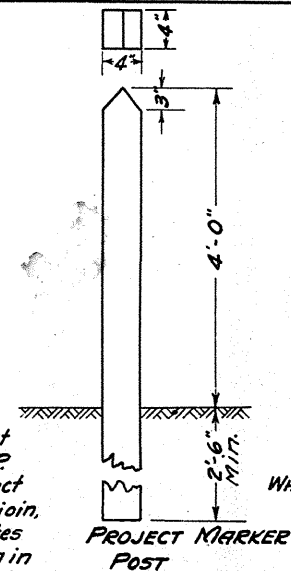
GENERAL NOTES: Posts to have minimum top diam. of 6". Tops of posts shall be neatly rounded. Untreated portion of all posts shall be shaved to the white before painting. All posts shall receive 3 coats of paint according to specifications and colors as shown. All posts shall be butt treated by the open tank process

### MARKER POSTS

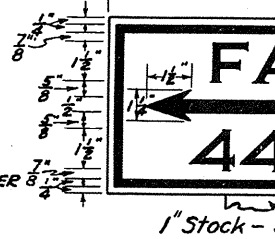


GENERAL NOTES: One F.A.P. plate shall be placed at the beginning and end of each F.A.P. on the right hand side as the project is entered. Where two projects adjoin, a single marker post and two plates shall be placed, each plate pointing in the direction of the project to which it refers. Where treated post is used a tight fitting Fiber Washer between post and plate shall be used.

### PROJECT MARKERS



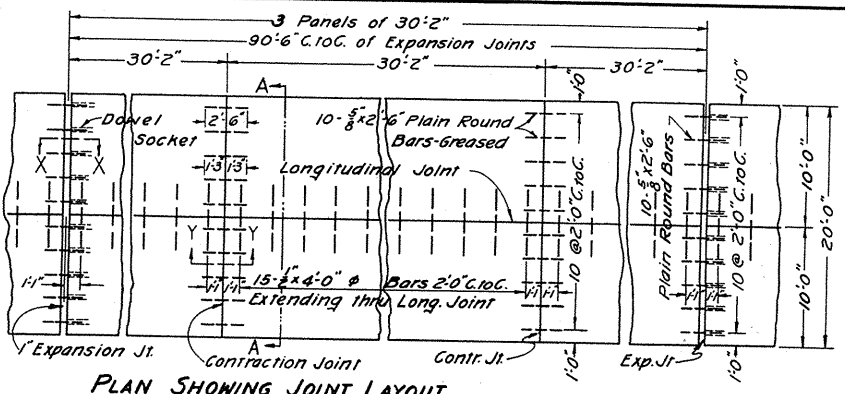
Use 2-1/4" x 2" lag screws with cut washers to fasten board to post



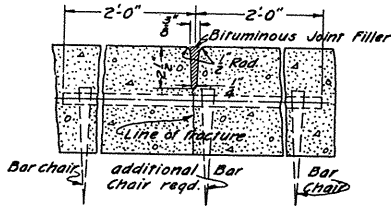
STANDARD MARKER & PROJECT

WISCONSIN HIGHWAY Recommended for Approval.

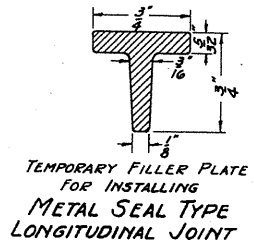
Approved: M. 97. Date: 2/10/32 ACT. STATE



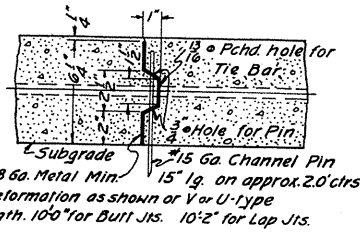
PLAN SHOWING JOINT LAYOUT



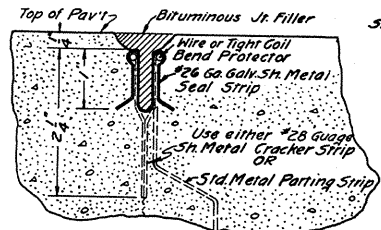
STD. IMPRESSED TYPE



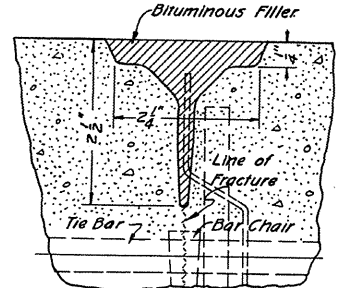
TEMPORARY FILLER PLATE FOR INSTALLING METAL SEAL TYPE LONGITUDINAL JOINT



STD. DEFORMED METAL PLATE TYPE

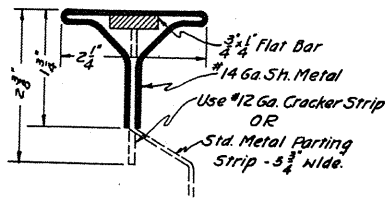


STD. METAL SEALED TYPE IMPRESSED OR DEFORMED PLATE TYPE IMPRESSED TYPE approved for use in transverse contraction joints



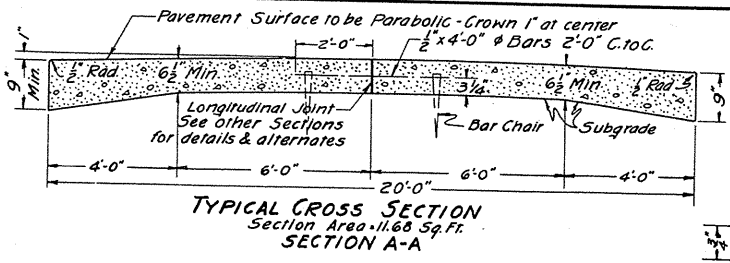
MARKER TYPE

Impressed or Def. Metal Plate Type

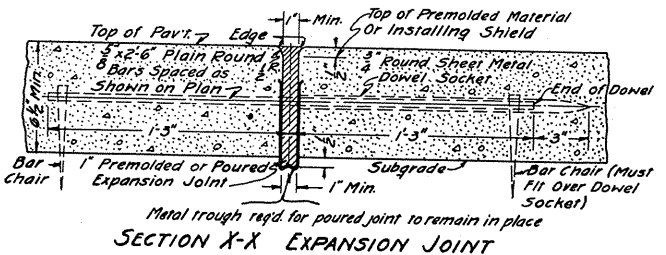


INSTALLATION FORM FOR MARKER TYPE

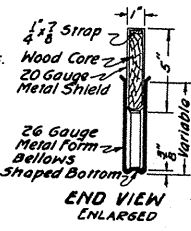
ALTERNATE TYPES LONGITUDINAL JOINTS



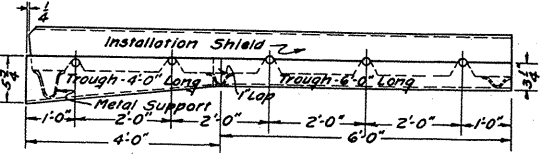
TYPICAL CROSS SECTION SECTION A-A



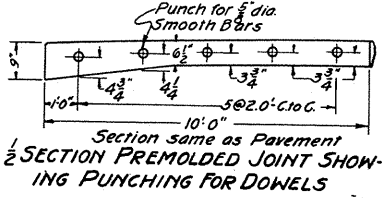
SECTION X-X EXPANSION JOINT



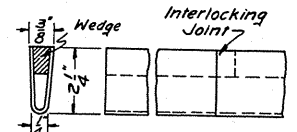
END VIEW ENLARGED



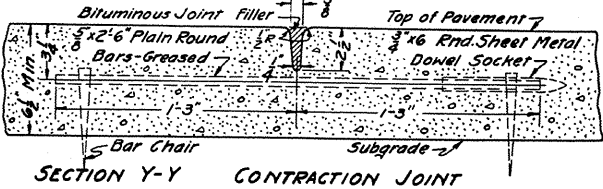
INSTALLATION FORM FOR POURED EXP. JT. Details of trough for poured joints to be approved by Engineer



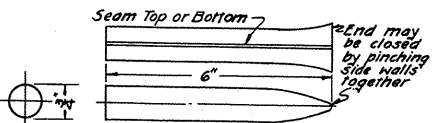
SECTION PREMOLDED JOINT SHOWING PUNCHING FOR DOWELS



TEMPORARY FILLER PLATE FOR CONTRACTION & LONGITUDINAL (Impressed Type) JOINT



SECTION Y-Y CONTRACTION JOINT



RND. SHEET METAL DOWEL SOCKET FOR CONTRACTION JOINT DOWEL

RND. SHEET METAL DOWEL SOCKET FOR EXPANSION JOINT DOWEL

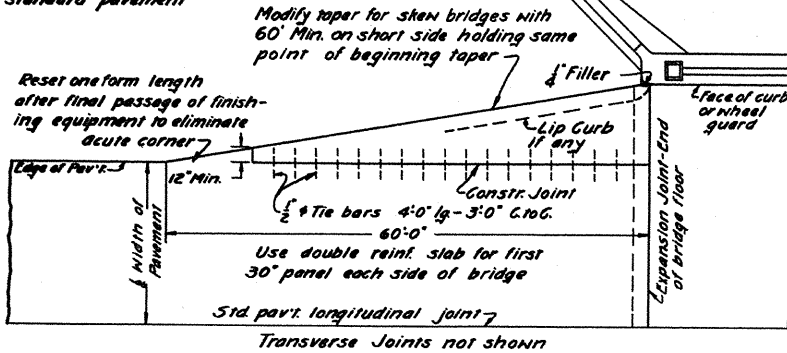
**CONSTRUCTION**  
Construction Details not covered in pertinent requirements of the Pavement shall be Edged as shown cut and any disturbance or brushed smooth.

Joint Spacing shall be as shown modification over Culverts, and at Surface Drains. Minimum not less than 20 nor more than 30 feet. Joints shall be clean cut at all other joints and at edges. Impressed Type Joints other subject to approval of the used.

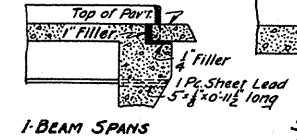
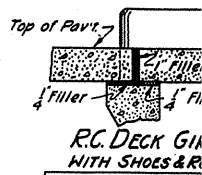
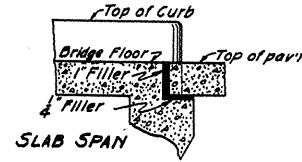
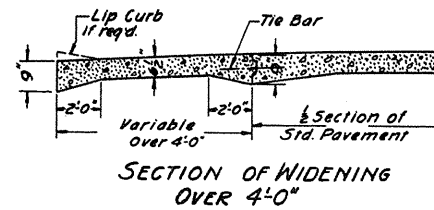
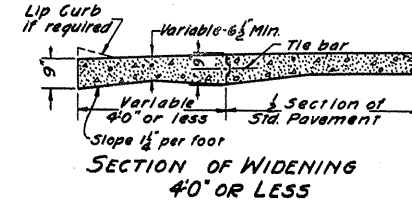
Bar Chairs for Tie and Dowel design that will hold bars in the type of soil or condition. Tie Bars thru Impressed Longitudinal Joints shall have an additional chair at center. Parting Strip, Metal Seal or Loose shall not extend thru Transverse Joints. Temporary Filler Plates and Shields shall be cleaned and oiled each time. Strips of deformed Metal Plate shall be placed adjacent to form Curbs, Gullies or Widening and The Tie Bars shall be bent in middle and installed in pavement groove of plate so bar can be removed of forms. Tie Bars shall be 1/2 inch round.

WISCONSIN HIGHWAY STANDARD  
20 FOOT PORTLAND CEMENT CONCRETE WITH JOINT L  
Recommended for approval: *A.J. H.*  
Approved: *M. W. S.*  
Date: 2/16/32 ACT. STATE

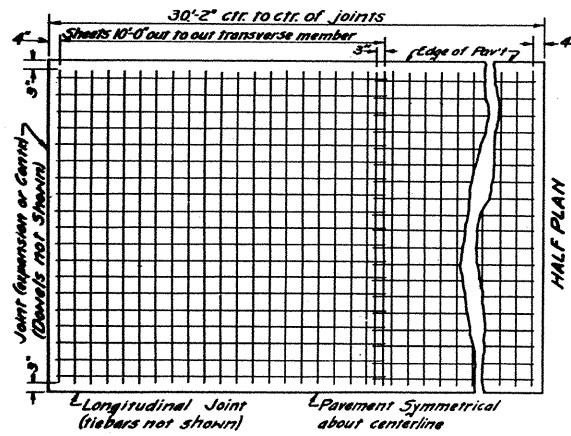
Bridge approach slabs to be widened adjacent to all structures over 60 ft span and in all cases where pavement surface drainage is carried on to or away from the structure. Widening will be paid for at the contract unit price per square yard for standard pavement.



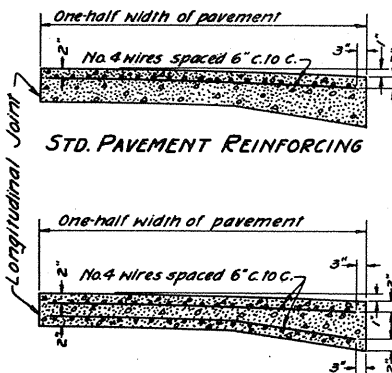
1/2 PLAN OF PAVEMENT WIDENING FOR BRIDGE APPROACHES



STEEL PLATE & STEEL TRUS



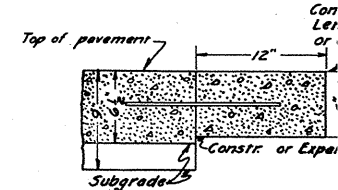
STEEL REINFORCEMENT FOR CONCRETE PAVEMENTS



REINFORCING FOR BRIDGE APPROACH SLABS AND OVER CULVERTS

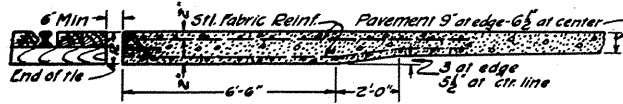
**CONSTRUCTION NOTES**  
**PAVEMENT REINFORCEMENT**  
 Details not covered on the plan shall conform to the pertinent requirements of the Standard Specifications of Road and Bridge Construction. The Diameter of all members shall be not less than 0.2253 inches (No. 4 gauge) and the fabric shall weigh not less than 58 pounds per 100 sq. feet. Longitudinal and Transverse members of the fabric shall be spaced six (6) inches center to center and be rigidly welded at all intersections. Spacing of Pavement Joints shall be modified that reinforced panels over culverts shall be centered over the structure and reinforced bridge approach slabs shall be full length (30'-2") The use of sleds to support the fabric will not be permitted.

**METHODS OF CONNECTING CONCRETE WITH BRIDGE STRUCTURE**

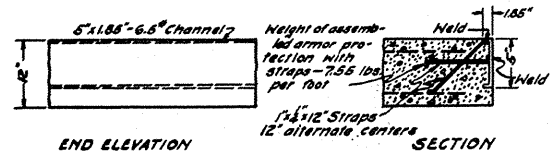


Construction joint with 6" x 3" dowels when placed at edge of pavement or standard expansion joint shall use standard dowels when placed at end of pavement.

CONCRETE HEADER



SECTION AT RT. ANGLES TO TRACK SHOWING REINFORCING



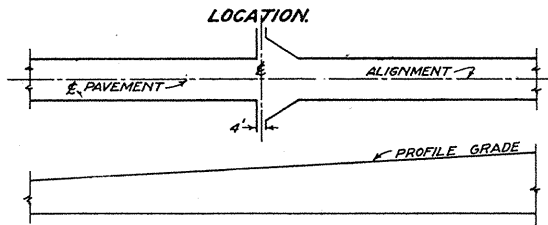
ARMOR PROTECTION AT R.R. CROSSING

**CONSTRUCTION NOTES**  
**ARMOR PROTECTION & REINFORCEMENT AT R.R. CROSSINGS**

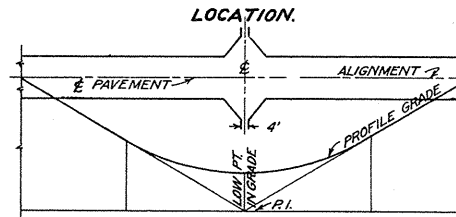
Protect ends of pavement at railroad crossings as shown. Pavement to be flat. Crown to be worked out in 25 feet. Additional concrete required in pavement to be converted into Sq. Yds. of standard concrete pavement and paid for as such. Armor protection shown to be measured by the pound and paid for at the contract unit price bid in the proposal for "Armor Protection" which price shall be full compensation for furnishing complete in place.

STANDARD BRIDGE APPROACH PAVEMENT AND CONCRETE WISCONSIN HIGHWAY DEPARTMENT  
 Recommended for approval:  
 Approved: \_\_\_\_\_  
 Date: 2/16/32

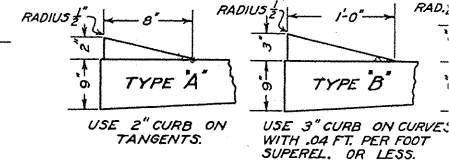
**SURFACE DRAIN ON GRADE.**



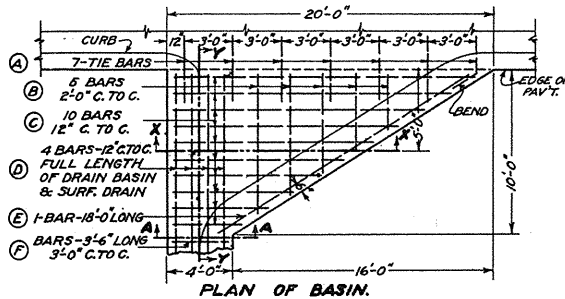
**SURFACE DRAIN AT LOW POINT.**



**TYPES OF LIP CURB**



**TYPE 10G.**



**BILL OF BARS (INCLUDING TIE BARS)**  
ALL BARS 1/2"

LET.	NUMBER & LENGTH
A	7 @ 4'-0"
B	1 @ 5'-6" / 1 @ 4'-6"
	1 @ 6'-6"
C	1 @ 18'-6" / 1 @ 10'-6"
	1 @ 17'-0" / 1 @ 9'-0"
	1 @ 15'-0" / 1 @ 7'-0"
	1 @ 13'-6" / 1 @ 5'-6"
	1 @ 12'-0" / 1 @ 4'-0"
D	4 @ 12'-0"
E	1 @ 17'-0"
F	3'-6" LENGTH.

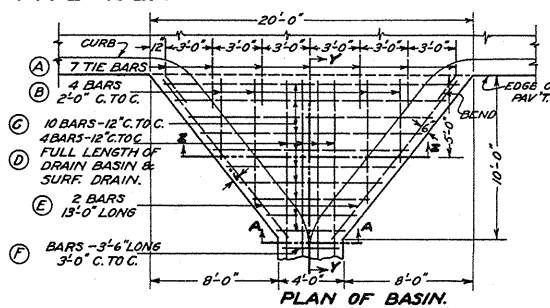
CONC. IN BASIN 2.76 CU.YDS.  
STEEL " 159.0 LBS.

**BILL OF BARS (INCLUDING TIE BARS)**  
ALL BARS 1/2"

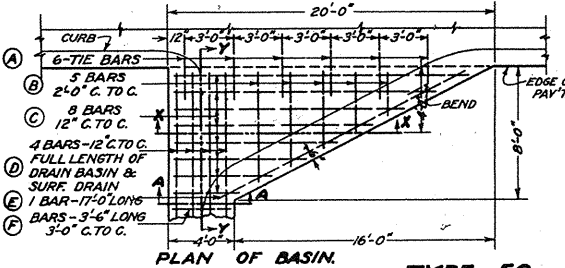
LET.	NUMBER & LENGTH
A	7 @ 4'-0"
B	2 @ 7'-0" / 2 @ 4'-6"
C	1 @ 18'-6" / 1 @ 10'-6"
	1 @ 17'-0" / 1 @ 9'-0"
	1 @ 15'-6" / 1 @ 7'-0"
	1 @ 13'-6" / 1 @ 5'-6"
	1 @ 12'-0" / 1 @ 4'-0"
D	4 @ 12'-0"
E	2 @ 13'-0"
F	3'-6" LENGTH.

CONC. IN BASIN 2.76 CU.YDS.  
STEEL " 160.0 LBS.

**TYPE 10LP.**



**TYPE 8G.**



**BILL OF BARS (INCLUDING TIE BARS)**  
ALL BARS 1/2"

LET.	NUMBER & LENGTH
A	6 @ 4'-0"
B	1 @ 5'-6" / 1 @ 3'-6"
	1 @ 4'-6"
C	1 @ 18'-6" / 1 @ 10'-0"
	1 @ 16'-0" / 1 @ 8'-0"
	1 @ 14'-0" / 1 @ 6'-0"
	1 @ 12'-0" / 1 @ 4'-0"
D	4 @ 10'-0"
E	1 @ 17'-0"
F	3'-6" LENGTH.

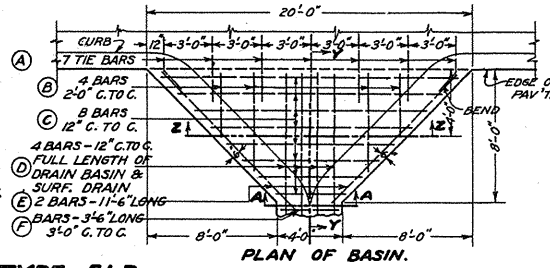
CONC. IN BASIN 2.21 CU.YDS.  
STEEL " 129.0 LBS.

**BILL OF BARS (INCLUDING TIE BARS)**  
ALL BARS 1/2"

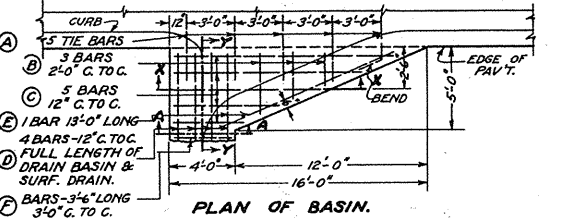
LET.	NUMBER & LENGTH
A	7 @ 4'-0"
B	2 @ 6'-0" / 2 @ 4'-0"
C	1 @ 18'-6" / 1 @ 10'-0"
	1 @ 16'-0" / 1 @ 8'-0"
	1 @ 14'-0" / 1 @ 6'-0"
	1 @ 12'-6" / 1 @ 4'-0"
D	4 @ 10'-0"
E	2 @ 11'-6"
F	3'-6" LENGTH.

CONC. IN BASIN 2.21 CU.YDS.  
STEEL " 135.0 LBS.

**TYPE 8LP.**



**TYPE 5G.**



**BILL OF BARS (INCLUDING TIE BARS)**  
ALL BARS 1/2"

LET.	NUMBER & LENGTH
A	5 @ 4'-0"
B	1 @ 4'-0" / 1 @ 3'-0" / 1 @ 2'-0"
C	1 @ 14'-0" / 1 @ 9'-6" / 1 @ 4'-0"
	1 @ 11'-6" / 6'-6"
D	4 @ 7'-0"
E	1 @ 12'-0"
F	3'-6" LENGTH.

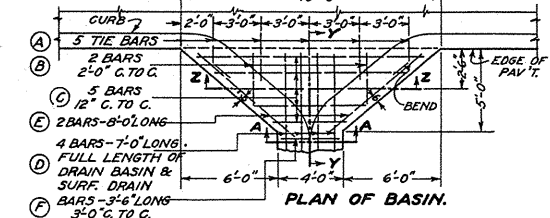
CONC. IN BASIN 1.17 CU.YDS.  
STEEL " 77.0 LBS.

**BILL OF BARS (INCLUDING TIE BARS)**  
ALL BARS 1/2"

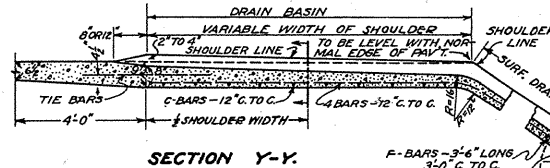
LET.	NUMBER & LENGTH
A	5 @ 4'-0"
B	2 @ 3'-0"
C	1 @ 14'-0" / 1 @ 9'-6" / 1 @ 4'-6"
	1 @ 11'-6" / 6'-6"
D	4 @ 7'-0"
E	2 @ 8'-0"
F	3'-6" LENGTH.

CONC. IN BASIN 1.17 CU.YDS.  
STEEL " 78.0 LBS.

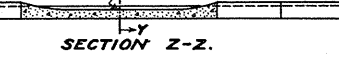
**TYPE 5LP.**



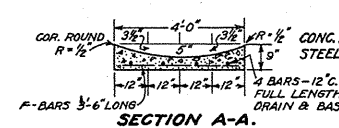
**SECTION X-X.**



**SECTION Y-Y.**

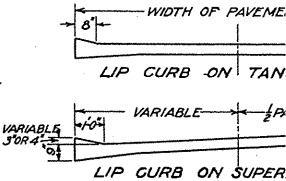


**SECTION Z-Z.**

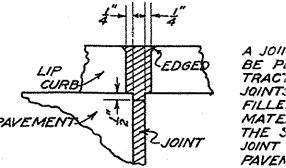


**SECTION A-A.**

**LIP CURB DE**



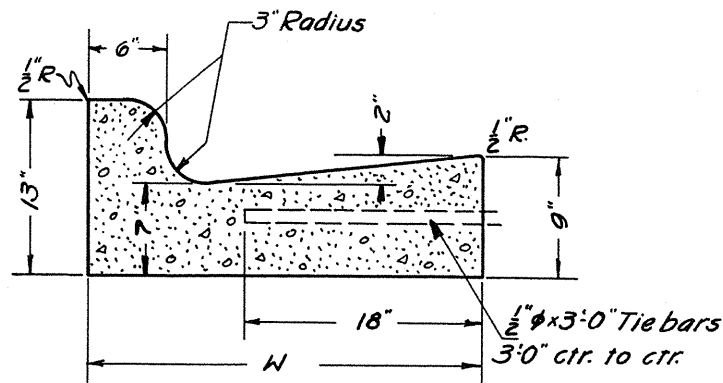
**DETAILS OF JOINT IN**



**CONSTRUCTION**

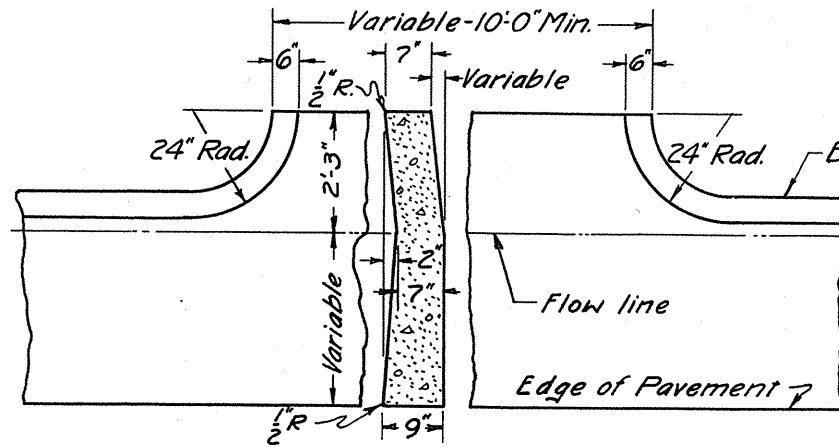
DETAILS NOT COVERED ON THE PLAN. THE PERTINENT REQUIREMENTS OF SPECIFICATIONS OF ROAD AND BRIDGE CONSTRUCTION SHALL BE ADJUSTED WHEN SURFACE OF PAVEMENT SLAB A SLOPED FROM A NORMAL THICKNESS EDGE TO 8" THICKNESS AT EDGE TO ALL REINFORCING AND TIE BARS TO

**LIP CURB.** WHEN DUE TO SUSPENSION OF POSITION OF THE LIP CURB CANNOT BE LIG TO WITH THE PAVEMENT SLAB, 5" TO TO THE PAVEMENT SLAB BY (A) INCH BY EIGHT (8) INCH DEFLECTOR SPIKES SPACED ONE (1) FOOT C.T. THREE (3) INCHES FROM THE EDGE. DO NOT EDGE PAVEMENT WHERE LIP CURB SHALL BE CAREFULLY EDGING TOOL AS SHOWN. **PLACING SURFACE DRAIN** THE FLOW LINE OF THE LIP CURB TOWARD THE SURFACE DRAIN BY PUMP BAST FORM AT THE POINT OF CONTACT BASIN WITH THE EDGE OF THE PAV'T. AFTER FINISHING EQUIP AND BEFORE FORM SHALL BE RESET WITH THE END OF FRONT EDGE & APPROX 1 IN. BELOW THE GRADE OF THE FRONT EDGE. THE RESULTING WIDENING SHALL BE FILLED WITH CONCRETE AND FINISHED AND THE LIP CURB CONSTRUCTED TO THE LINE THUS FORMED



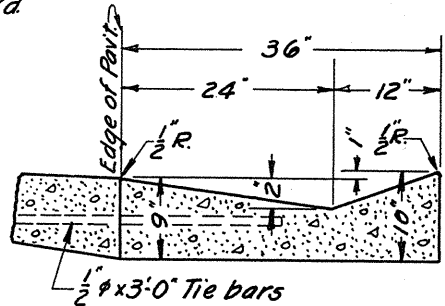
W=24" for Narrow Type Concrete per Lin.Ft. = 0.056 Cu.Yd.  
 W=30" for Standard Type Concrete per Lin.Ft. = 0.068 Cu.Yd.  
 W=36" for Wide Type Concrete per Lin.Ft. = 0.081 Cu.Yd.

**STANDARD CONCRETE COMBINATION CURB AND GUTTER**

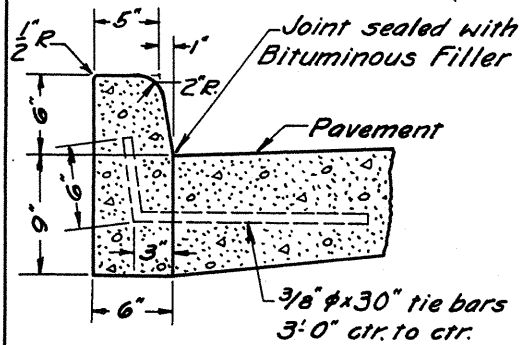


**PLAN OF APPROACH THROUGH COMBINATION CURB AND GUTTER**

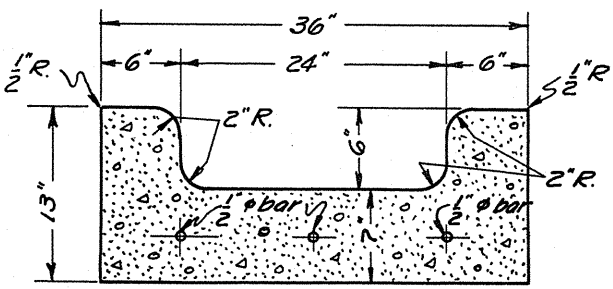
**CONSTRUCTION**  
 Unless otherwise specified, curb, gutter or curb and gutter shall be tied to the pavement reinforcement bars as shown. All concrete shall conform to the requirements specified in the Standard Specifications for Structures and Construction. Details of construction shall be in accordance with the Standard Specifications.



**STD. V-TYPE CONCRETE GUTTER**  
 Concrete per Lin. foot = 0.0756 Cu.Yd.



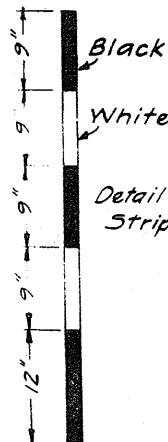
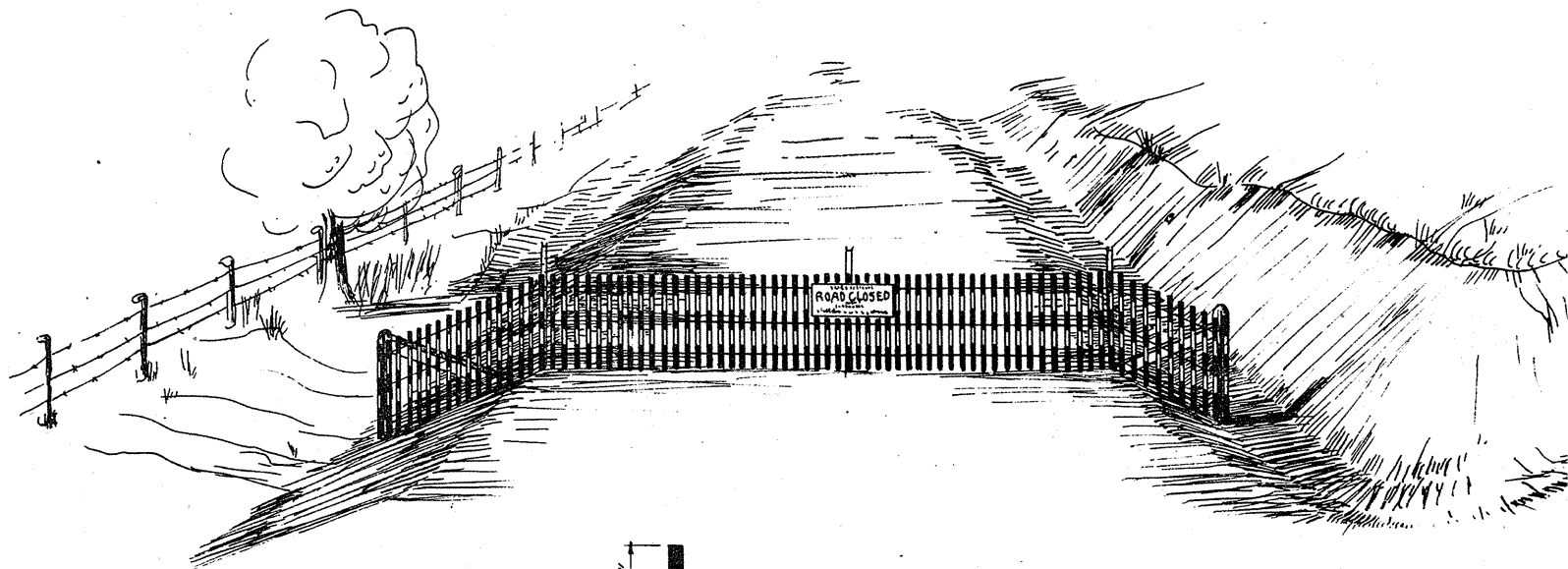
**STANDARD CONCRETE CURB**  
 Concrete = 0.022 Cu.Yds. per lin. ft.



**STANDARD FLUME TYPE CONCRETE GUTTER**  
 Concrete per lin. ft. = 0.083 Cu.Yd.

WISCONSIN HIGHWAY DEPARTMENT  
**STANDARD CURB, GUTTER AND COMBINATION CURB AND GUTTER**  
 Recommended for Approval: *[Signature]*  
 Approved: *[Signature]*  
 ACT. STATE HIGHWAY ENGINEER  
 Date: 2/16/32

Section of Snow Fence to be painted alternately black and white as shown below.  
 Length to be sufficient to reach from ditch to ditch.  
 To be erected on channel iron posts; Ends to be supported by 2 cedar posts & securely  
 Standard "Road Closed" Sign (W.H.C.-R-6) to be erected on fence at top on  
 centerline of road.



Detail Showing Method of  
 Striping For 4' Snow  
 Fence.

## CONSTRUCTIVE BARRICADE

WISCONSIN HIGHWAY COMM.

Approved:

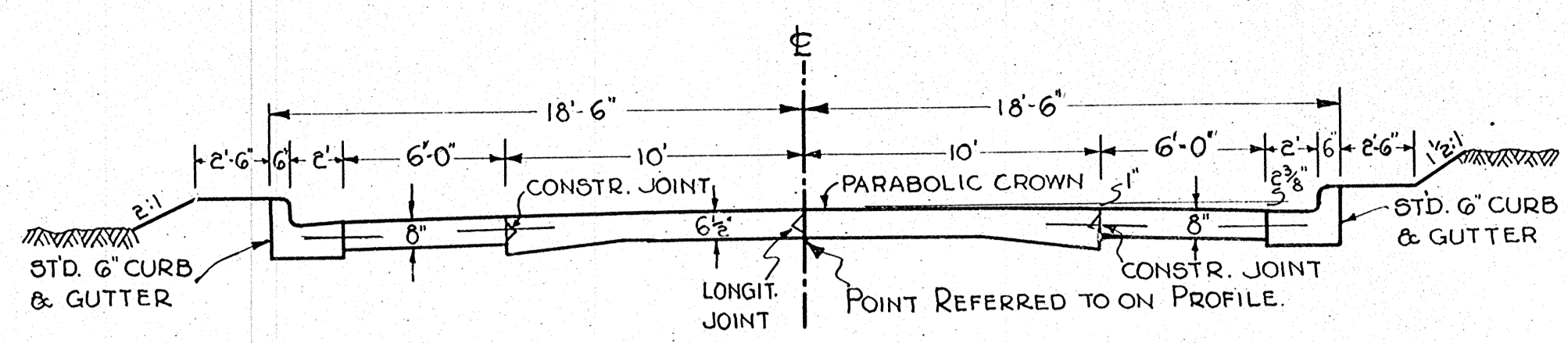
*M. W. Jorsett*

State Highway Engineer

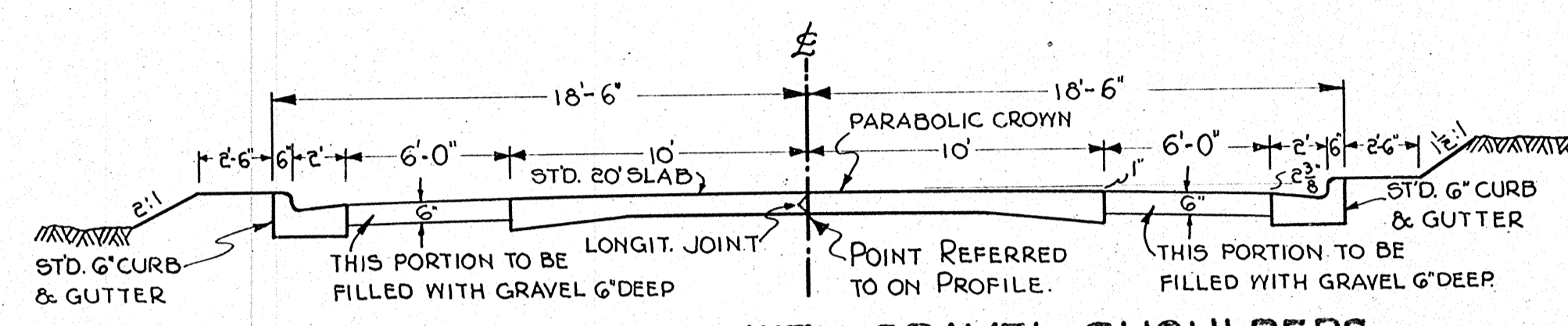
Drawn 2-3-31 - F.L.B. -

C-48

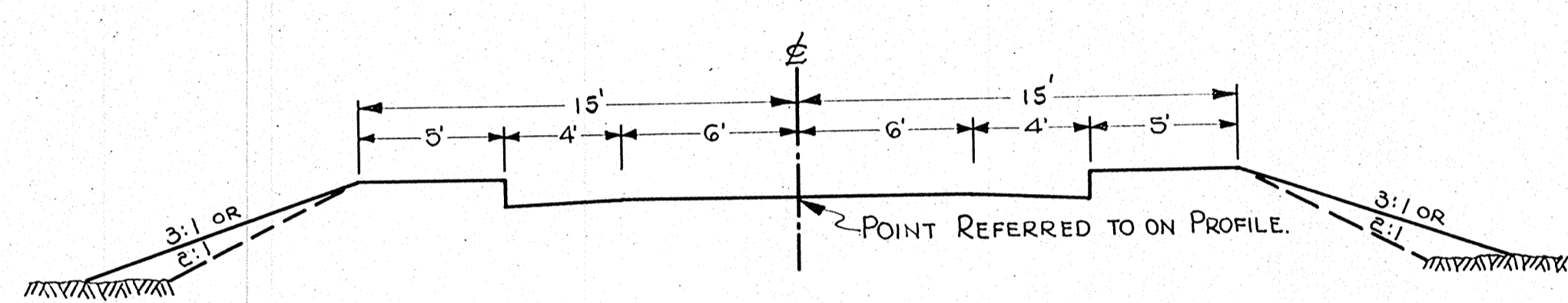
DIVISION JOB NO. 3501



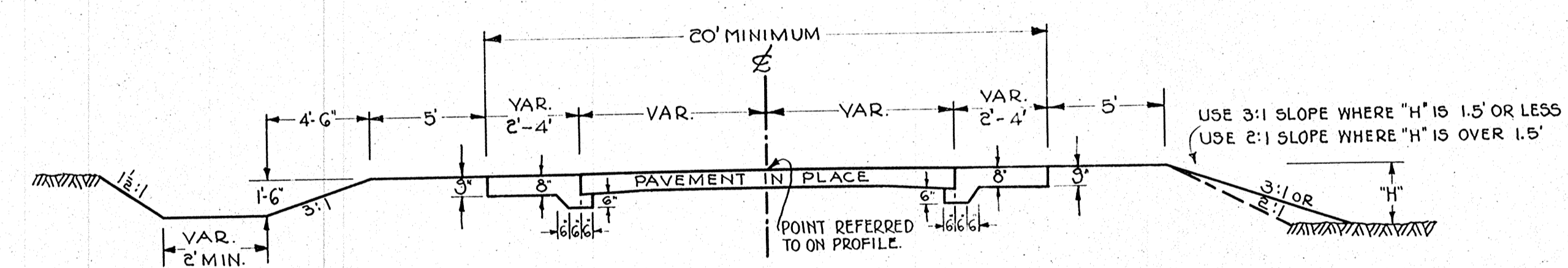
**SPECIAL PAV'T. SECTION WITH CURB & GUTTER**  
STA. 0+00 TO STA. 9+00



**20FT. PAV'T. SECTION WITH GRAVEL SHOULDERS AND CURB & GUTTER.**  
STA. 9+00 TO STA. 30+00



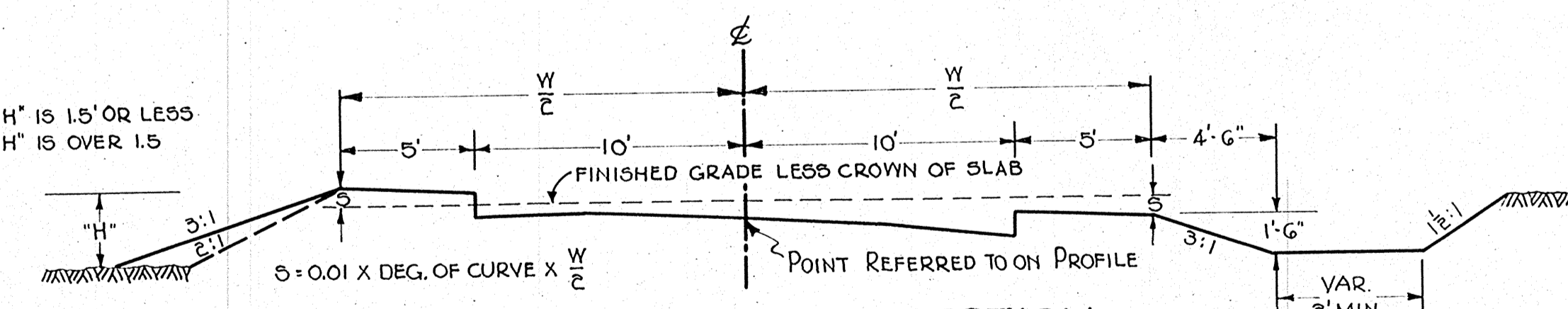
**PAV'T. SECTION WITH SHOULDERS**  
STA. 30+00 TO STA. 33+25.1



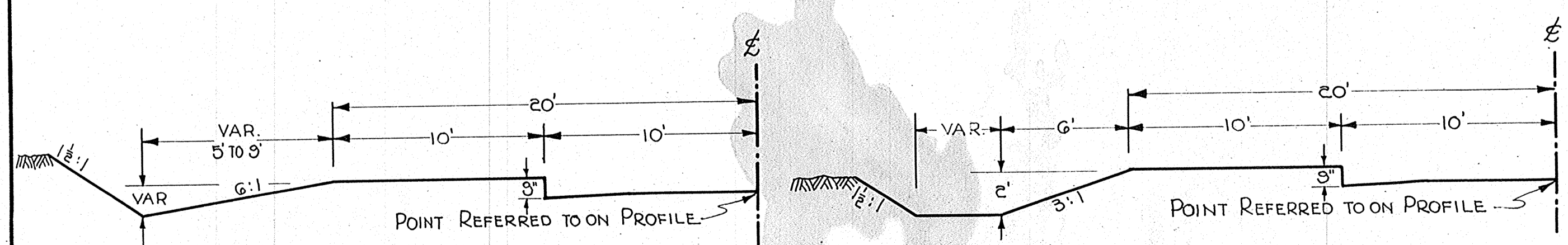
**PAV'T. WIDENING SECTION**

STA. 33+25.1 TO 34+50 LT. & RT.  
STA. 34+50 TO 35+35 RT.  
STA. 35+00 TO 43+50 LT.  
STA. 50+50 TO 131+00 LT.  
STA. 160+00 TO 220+00 RT.  
STA. 228+41 TO 254+98.7 RT.

USE 3:1 SLOPE WHERE "H" IS 1.5' OR LESS  
USE 2:1 SLOPE WHERE "H" IS OVER 1.5'

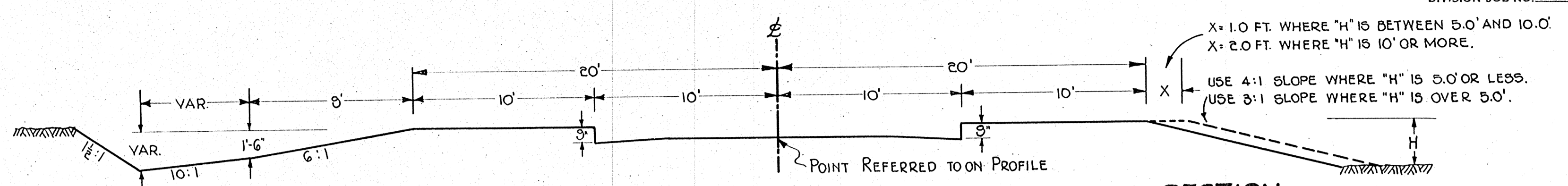


**SUPERELEVATED CURVE SECTION**  
STA. 220+089 TO STA. 228+41



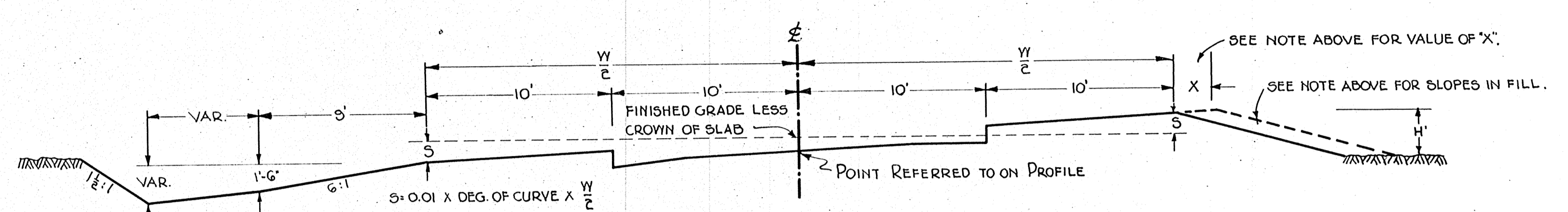
**ONE-HALF SECTION SHOWING SPECIAL DITCH**  
STA. 254+98.7 TO STA. 261+75

**SPECIAL DITCH SECTION ON LEFT**  
STA. 261+75 TO STA. 269+00

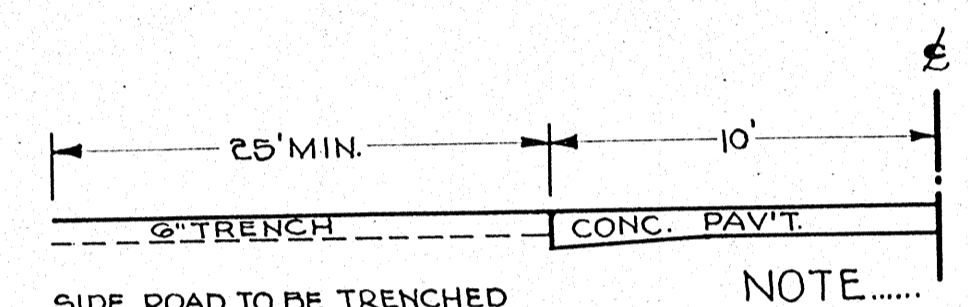


**ONE HALF STANDARD CUT & ONE HALF STANDARD FILL SECTION**  
STA. 269+00 TO STA. 279+30

X = 1.0 FT. WHERE "H" IS BETWEEN 5.0' AND 10.0'  
X = 2.0 FT. WHERE "H" IS 10' OR MORE.  
USE 4:1 SLOPE WHERE "H" IS 5.0' OR LESS.  
USE 3:1 SLOPE WHERE "H" IS OVER 5.0'.



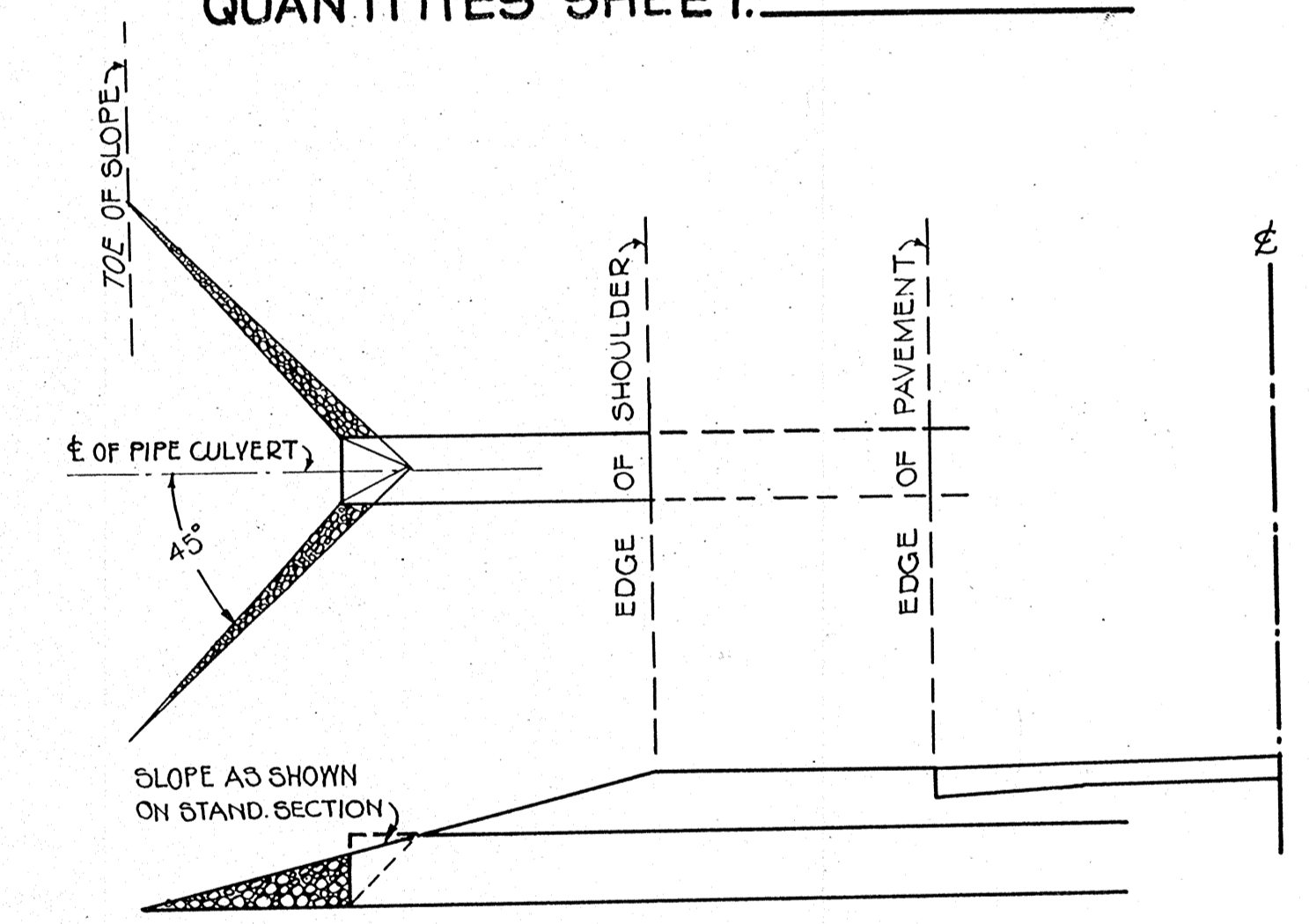
**STANDARD SUPERELEVATED SECTION**  
STA. 279+30 TO STA. 288+58



SIDE ROAD TO BE TRENCHED 6" DEEP TO A DESIGNATED WIDTH & BACKFILLED WITH APPROVED GRAVEL.  
NOTE..... TRENCHING AS SHOWN ON PLAN SHEETS IS INCLUDED IN EXCAVATION ON ESTIMATE SHEET.

**SECTION SHOWING TRENCHING ON SIDE ROADS**

NOTE: LOCATIONS OF STANDARD 20FT. P.C. CONC. PAV'T. NOTED ON MISCL. QUANTITIES SHEET.

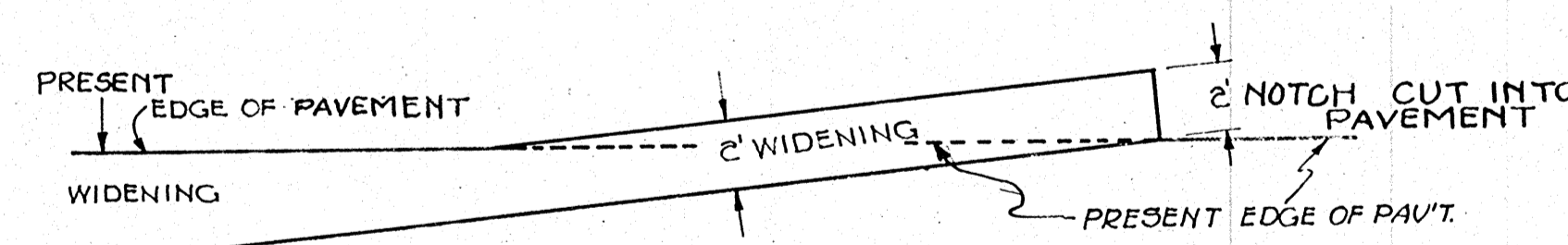


**DETAIL SHOWING RIP RAP AT PIPE CULVERTS UNDER CENTERLINE**

**STANDARD NOTE**

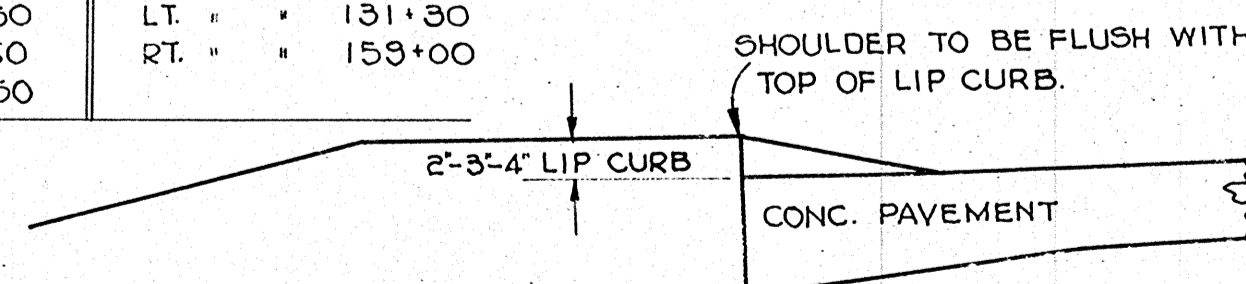
STANDARD STRUCTURAL PLANS AND REVISED STANDARD DETAILS AS APPROVED BY THE U.S. BUREAU OF PUBLIC ROADS ON MAY 13, 1928 AND AS SUBMITTED FOR 1933 WILL BE USED ON THIS PROJECT. APPROVED DETAIL DRAWINGS ARE AS FOLLOWS:

- MARKER POSTS & PROJ. MARKERS C-36
- BR. APPR. SLABS, PAV'T. REINF., CONC. HDR. BLOCKS C-39-1
- 20' P.C. CONC. PAV'T. WITH JOINT DETAILS C-40
- COMBINATION CURB & GUTTER C-44-1
- CONSTRUCTION BARRICADE C-48
- CONC. SURF DRAINS & LIP CURB C-42-1



**DETAIL OF NOTCH FOR WIDENING**  
-ELIMINATING FEATHER-EDGING OF CONCRETE-

- LT. OF STA. 34+50
- RT. " 35+35
- LT. " 38+50
- LT. " 48+60
- LT. " 50+50
- LT. OF STA. 114+00
- LT. " 115+50
- LT. " 131+30
- RT. " 159+00



**LIP CURB AND SHOULDER DETAIL**

TYPICAL CROSS SECTION FOR  
20FT. TO 36FT. CONCRETE PAV'T.  
&  
30FT. TO 40FT. ROADW'Y.  
WISCONSIN HIGHWAY COMMISSION  
MADISON WISCONSIN



# ESTIMATE OF QUANTITIES

AS AMENDED, APPROVED 8-16-33  
LABOR PROVISIONS APPROVED 8-31-33

THIS PROJECT IS TO BE EXECUTED UNDER WISCONSIN STANDARD SPECIFICATIONS AS APPROVED BY THE U. S. BUREAU OF PUBLIC ROADS ON JULY 27, 32

FED. ROAD DIST. NO.	STATE	U.S.P.W. PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WISC.	N.R.H. 383-B	3	50

MILE NO.	STATION TO STATION	NET LENGTH OF CENTER LINE	EARTH WORK															BASE COURSES				SURFACE COURSES										
			CLEAR-ING	GRUBB-ING	EXCAVATION								SUR-FACE STONE	INTER-CEPT G DITCH	SPEC'L DITCH-ES	FINE GRAD-ING	FINISH-ING SH'L'D'R	SH'L'D'R EMB'K-MENT	HAUL SH'L'D'R EMB'K'T	SH'L'D'R SURF'G	SEED-ING	BROK'N STONE	P.C. CONC. PAV'T. IN WIDEN'G	PORT'D CEMENT CONC. PAV'T.	LIP CURB			CONC. SURF. DRAINS	CONC. HEAD-ERS	BAR STEEL REINF.	STEEL FABRIC REINF.	GRAVEL SURF-ACING
					EARTH	LOOSE ROCK	SOLID ROCK	MARSH	EARTH CHAN-NEL	EARTH BOR-ROW	3.1	3.2													3.3	3.4	3.5					
	ITEM NO.	LIN. FT.	ACRE	ACRE	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	C. Y.	L. FT.	L. FT.	L. FT.	C. Y.	L. FT.	LB.	S. Y.	CY.	
1.	0+00 - 53+00	4760.0			5227		1241			213																						
2.	53+00 - 106+00	5300.0			1113		33			555																						
3.	106+00 - 131+50	2400.0			295					566																						
4.	159+00 - 212+00	5300.0			1574		47			198																						
5.	212+00 - 265+00	5300.0	0.61	0.61	2480		599			67																						
6.	265+00 - 288+58	2358.0			10262		143																									
<b>TOTAL</b>		<b>25418.0</b>	<b>0.61</b>	<b>0.61</b>	<b>20,351</b>		<b>2063</b>			<b>15,299</b>	<b>25</b>																					

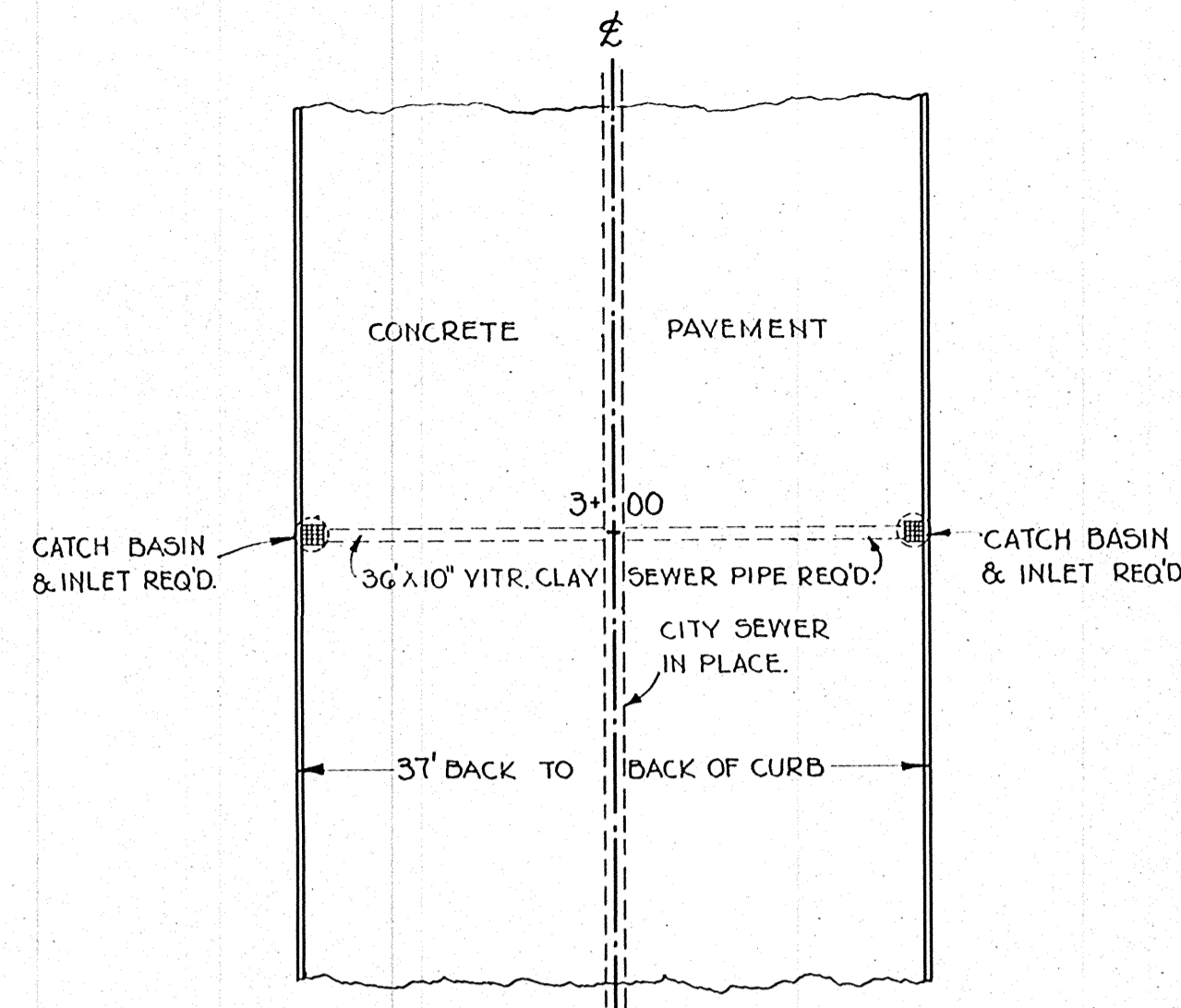
MILE NO.	STRUCTURES OVER 20'-0" SPAN													STRUCTURES 20'-0" SPAN OR LESS BUT OVER 6'-0" SPAN										CULVERTS 6'-0" SPAN AND UNDER										
	EXCAV-ATION	UNTR'D TIMBER PILING DELIV'D.	UNTR'D TIMBER PILING DRIVEN	CHAN-NEL CHANGE	CONC. MASON-RY	BAR STEEL REINF.	STEEL FABRIC REINF.	STRUCT-URAL STEEL	CAST STEEL	SHEET LEAD	ZINC PLATE	FLOOR DRAINS	PIPE RAILING WITH POSTS	WATER PROOF-ING	EXCAV-ATION	UNTR'D TIMBER PILING DELIV'D	UNTR'D TIMBER PILING DRIVEN	CONC. MASON-RY	BAR STEEL REINF.	STEEL FABRIC REINF.	STRUCT-URAL STEEL	SHEET LEAD	FLOOR DRAINS	PIPE RAILING WITH POSTS	WATER PROOF-ING	EXCAV-ATION	CONC. MASON-RY	BAR STEEL REINF.	STEEL FABRIC REINF.	STEEL	CATCH BASIN INLETS	WATER PROOF-ING		
																																	C. Y.	L. FT.
1					4.7.1	4.8.1	4.8.5	4.13.1	4.13.2					4.19.1	4.4.2	4.5.1	4.5.2	4.7.2	4.8.2	4.8.5	4.13.1	4.13.7				4.25.1	4.4.3	4.7.3	4.8.3	4.8.5				4.25.1
2																																		
3	80				6.51	7080		160						2													10	3.6	340					125
4																																		
5	290			850	176.6	9030		30100			167			4																				
6																																		
<b>TOT.</b>		<b>370</b>		<b>850</b>	<b>241.7</b>	<b>16110</b>		<b>30260</b>			<b>167</b>			<b>6</b>													<b>105</b>	<b>52.1</b>	<b>5660</b>			<b>330</b>	<b>2</b>	<b>1375</b>

MILE NO.	CULVERT PIPE													INCIDENTAL CONSTRUCTION												
	C.G.S.M.P.			SIDE ROAD PIPES 18"	PIPES UNDER			CORR. GALV. SHEET MET.	RIP-RAP	TILE UNDER DRAINS	10" IN. STORM SEWER	CATCH BASINS COMPLETE	MAN-HOLES	INLETS	ARMOR PROT-ECTION	COMB. CONC. CURB AND GUTTER		WIRE ROPE GUARD FENCE		AN-CHOR-AGES	MARK-ER POSTS	RESET GUARD FENCE POSTS	R.O.W. MARKER POSTS	PROJ. MARK-ERS		
	PRIV. ENTR. PIPES 18"				24"	36"	42"									L. FT.	L. FT.	L. FT.	L. FT.						L. FT.	L. FT.
1.	4.31.8		4.31.8					5.1.1	5.3.1	5.2.1	5.4.1	5.4.11	5.4.21	4.13.1	5.5.5	5.5.5	5.7.2	5.7.3	5.9.1	5.P.						
2.		120						6.5												2						
3.		20																		6						
4.		60	36					4.5												4						
5.		140	36					1.5												2		10				
6.		100						0.75												1		12				
<b>TOT.</b>		<b>440</b>	<b>72</b>	<b>141</b>	<b>9</b>			<b>13.25</b>		<b>91</b>	<b>5</b>			<b>1275</b>	<b>6000</b>				<b>15</b>	<b>13</b>	<b>22</b>					

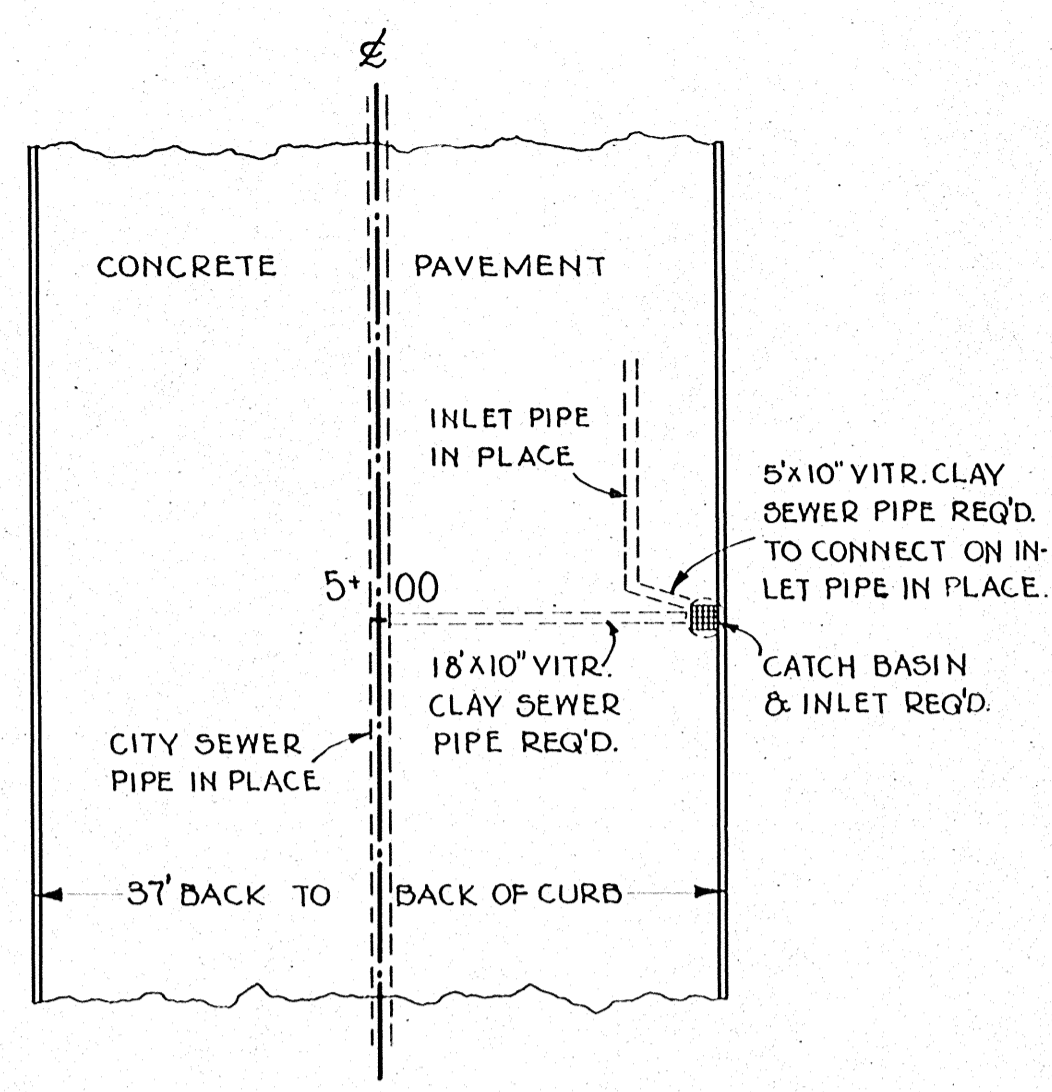


FED. ROAD DIST. NO.	STATE	U.S.P.W. PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	N.R.H. 393-B	3-B	56

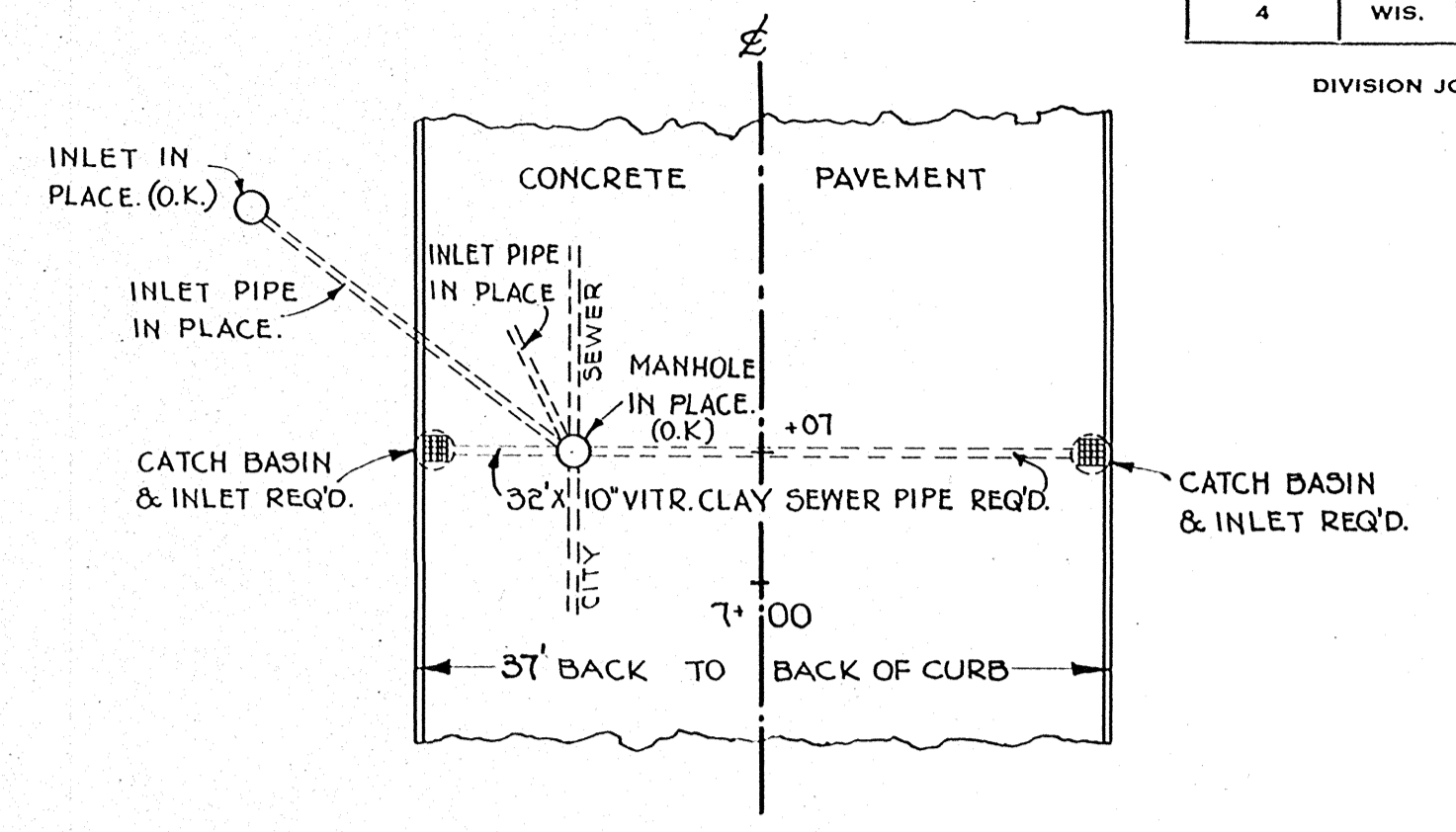
DIVISION JOB NO. 3501



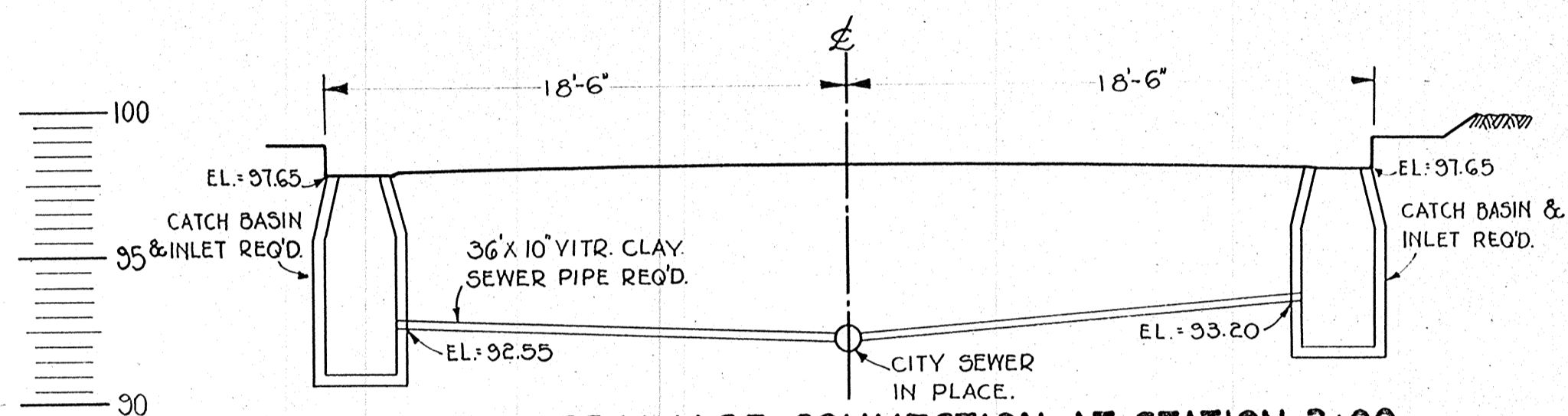
DETAIL AT STA. 3+00



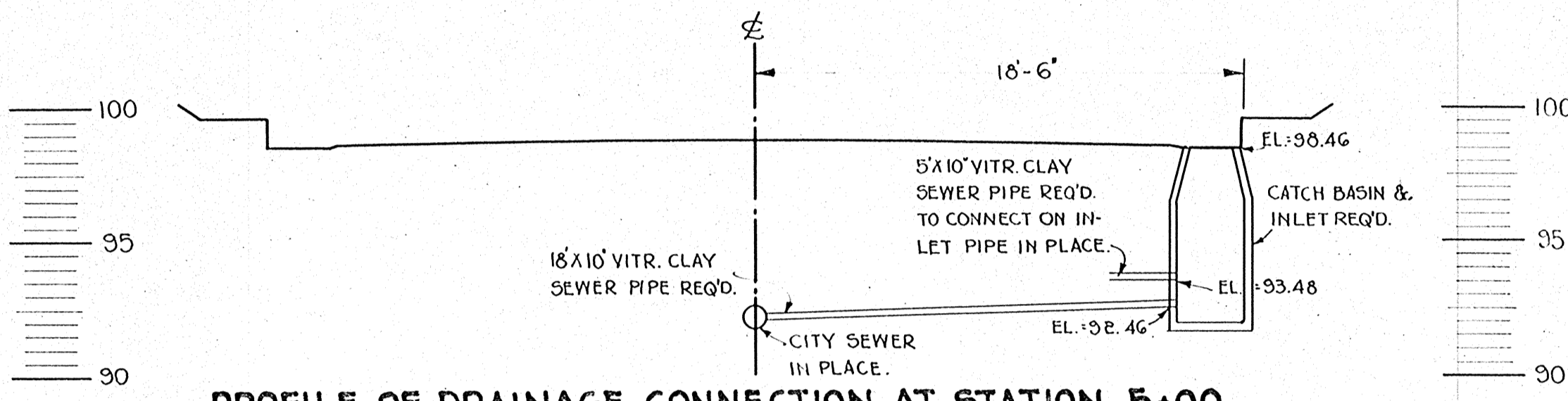
DETAIL AT STA. 5+00



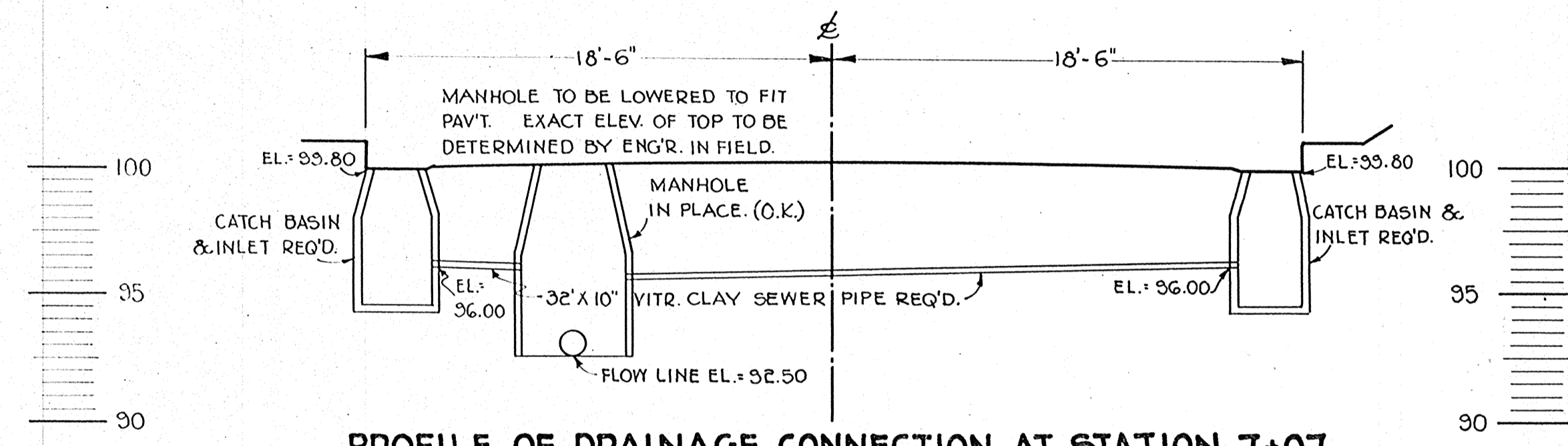
DETAIL AT STA. 7+07



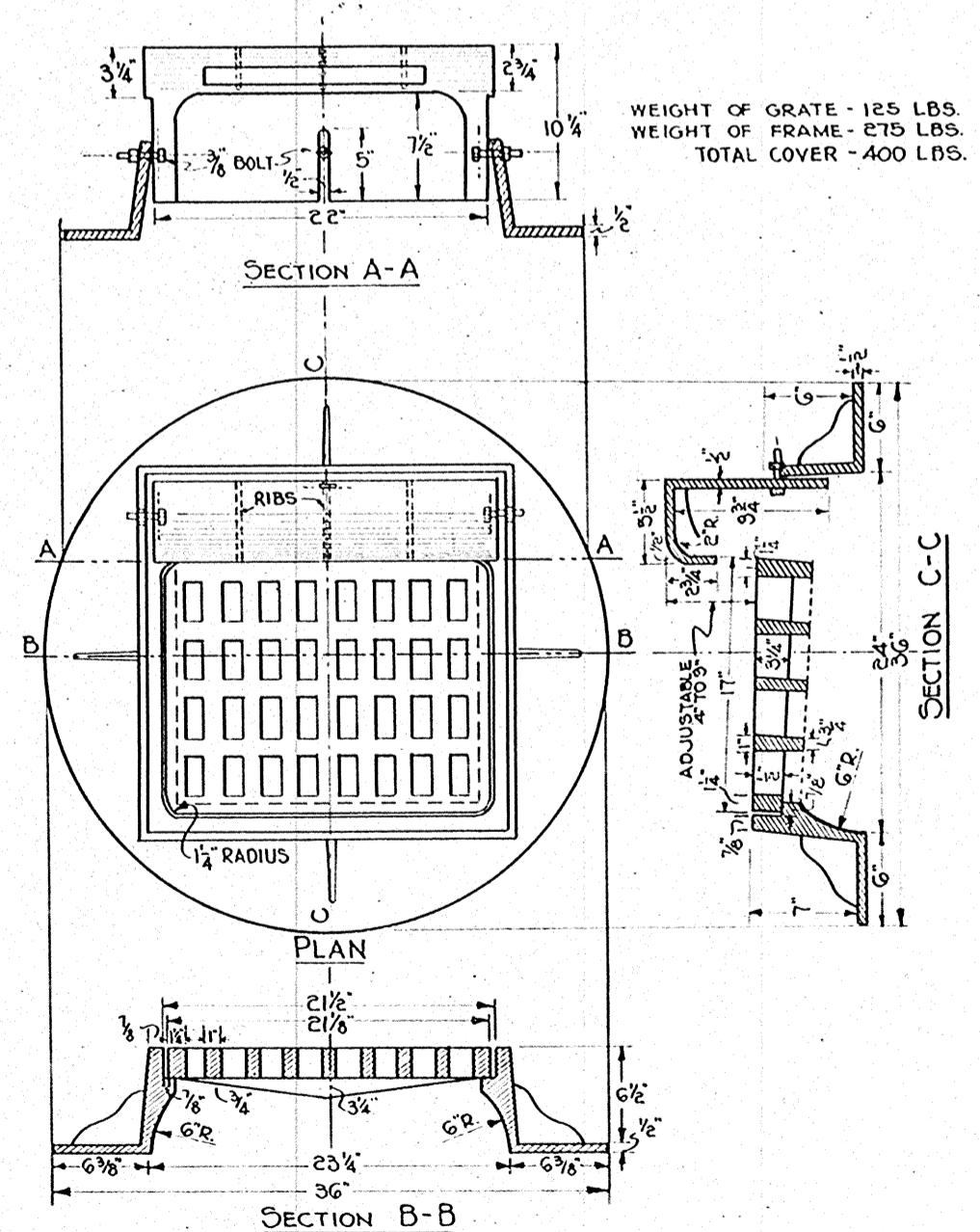
PROFILE OF DRAINAGE CONNECTION AT STATION 3+00



PROFILE OF DRAINAGE CONNECTION AT STATION 5+00

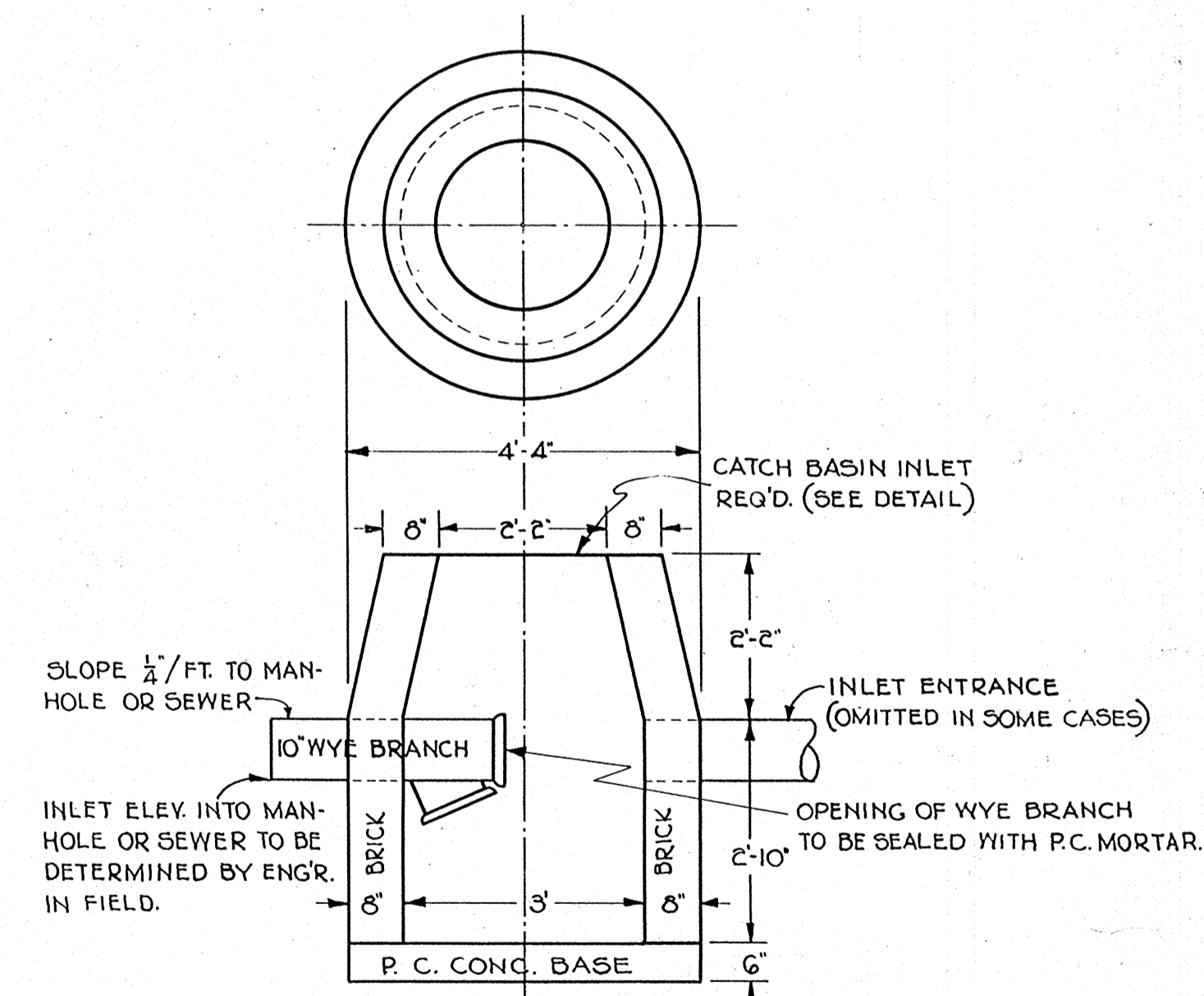


PROFILE OF DRAINAGE CONNECTION AT STATION 7+07



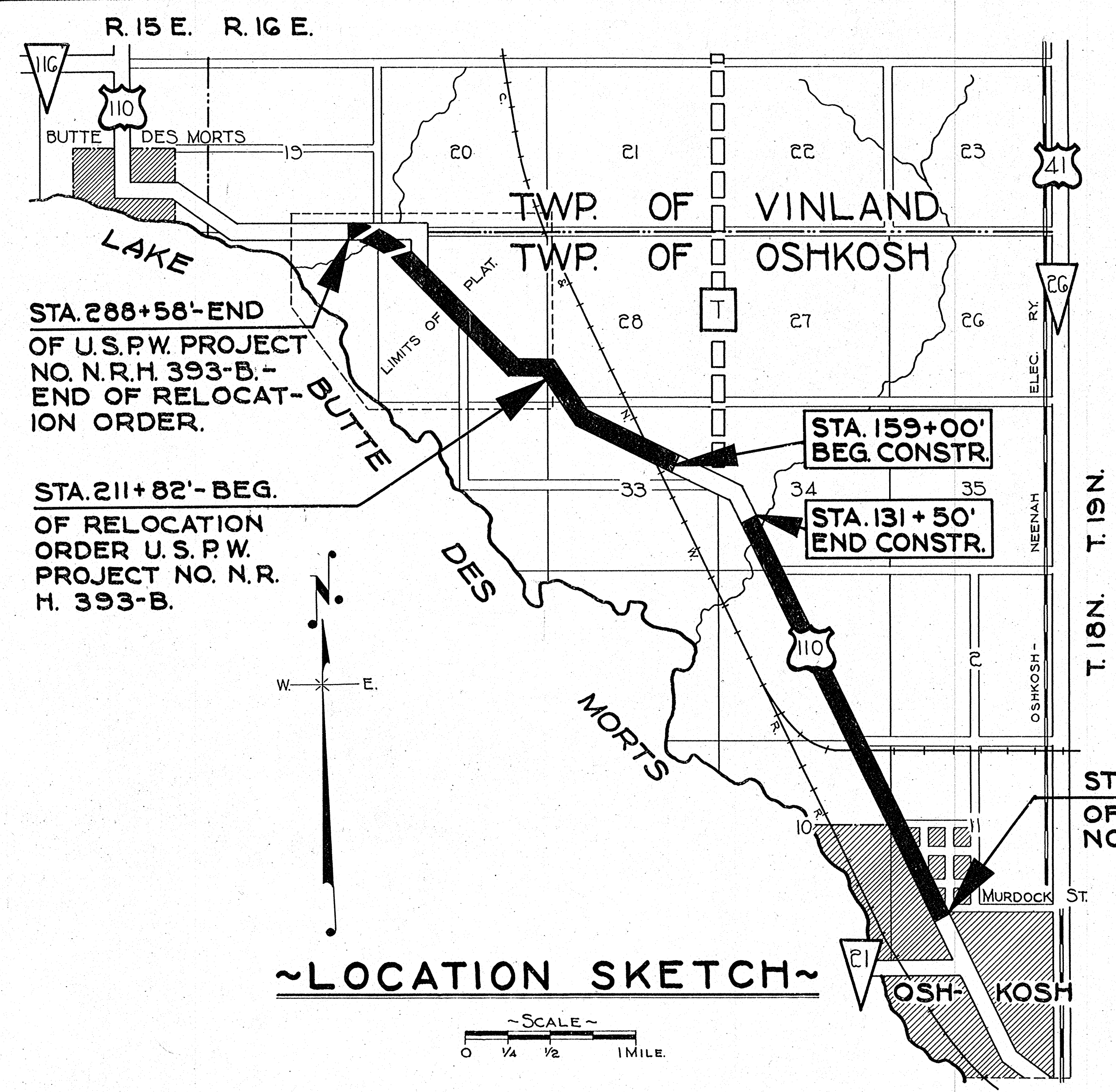
DETAIL OF GRATE TO BE USED

EQUIVALENT TO NO. 40 OF AYLWARD & SONS CO. NEENAH, WIS.



DETAIL OF CATCH BASIN

SPECIAL DETAIL SHEET  
 SHOWING  
 PROPOSED DRAINAGE CONNECTIONS  
 AND  
 CATCH BASIN DETAILS  
 WISCONSIN HIGHWAY COMMISSION  
 MADISON WISCONSIN



~LOCATION SKETCH~

NO.	OWNER	ADDRESS	AC'S	DESCRIPTION	MORTGAGEE	ADDRESS	R/W	FEN.	DAM.	TOT.	PAID	REMARKS
1	G. PLUMMER		0.15	NE 1/4, SE 1/4, SEC. 29, T. 19N. R. 15E.								
2	A. POMMERENING		0.01	SW 1/4, NW 1/4, SEC. 29, " "								
3	MRS. E. LUEBKE		5.35	SE 1/4, NW 1/4, SEC. 29, " "								
4	E. KROCK		0.01	NE 1/4, NE 1/4, SEC. 30, " "								

PLAT OF RIGHT OF WAY REQUIRED  
 U. S. PUBLIC WORKS PROJECT NO. N.R.H. 393-B.  
 OSHKOSH-BUTTE DES MORTS ROAD  
 U.S.H.NO. 110. WINNEBAGO COUNTY

FED. ROAD DIST. NO.	STATE	U. S. P. W. PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	393-B	5	56

DIVISION JOB NO. 3501

STA. 288+58' END OF U. S. P. W. PROJECT NO. N. R. H. 393-B. END OF RELOCATION ORDER.

PI: 284+07.2'  
 $\Delta$ : 36°-17'  
 D: 5°-00'  
 T: 375.6'  
 LC: 725.7'  
 PC: 280+31.6'  
 PT: 287+57.3'  
 R: 1146.3'

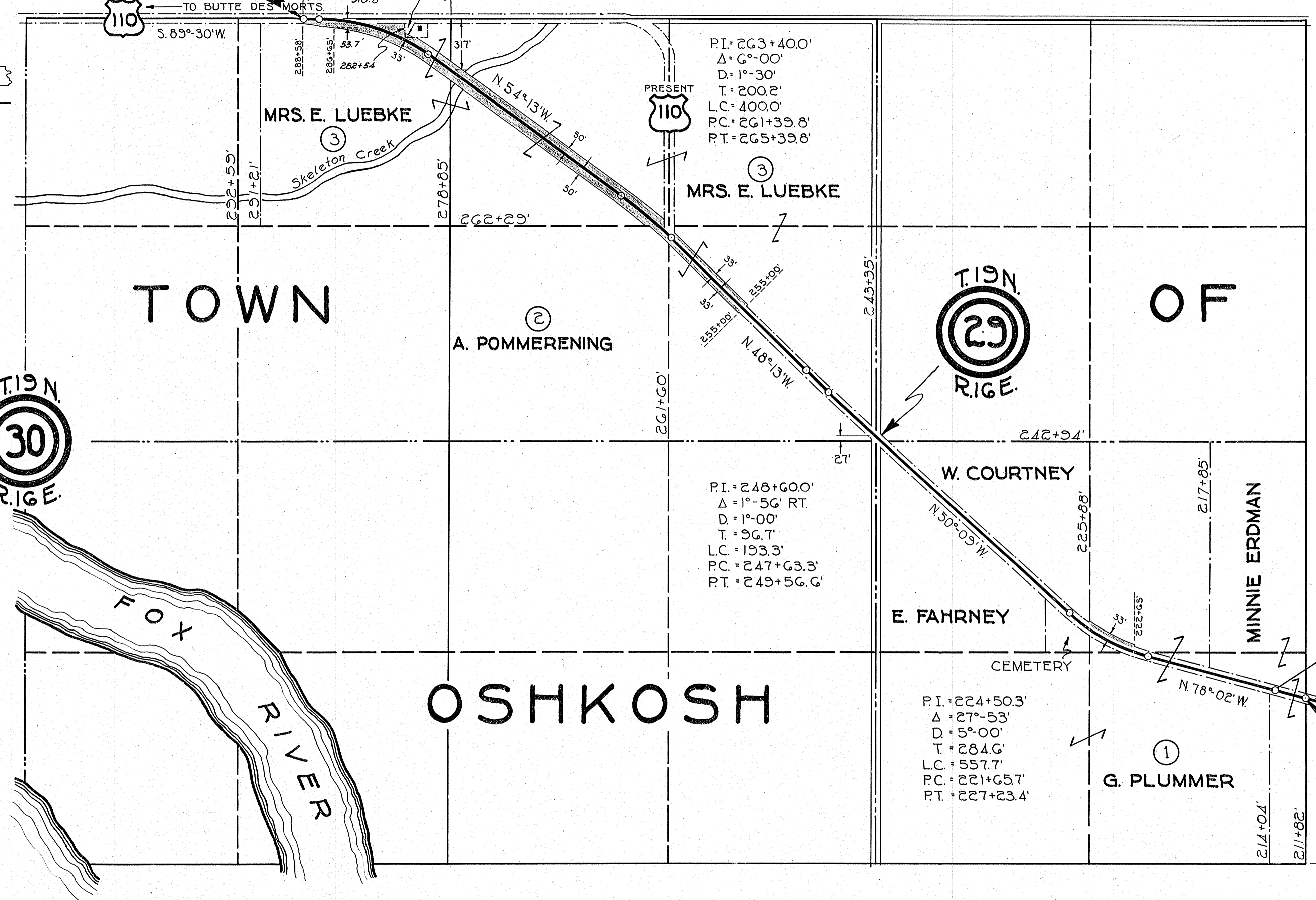
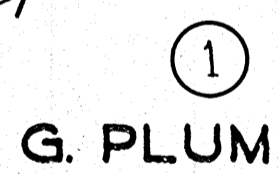
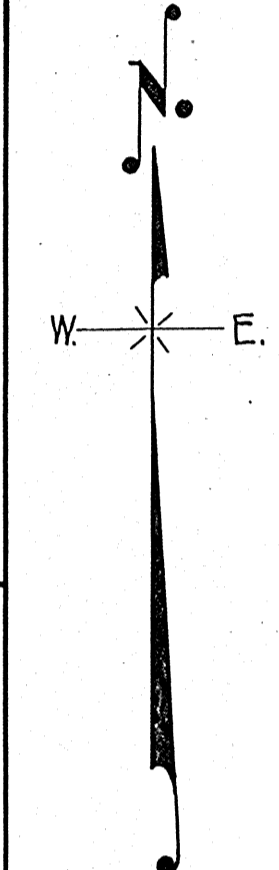
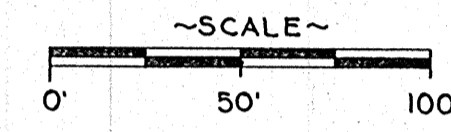
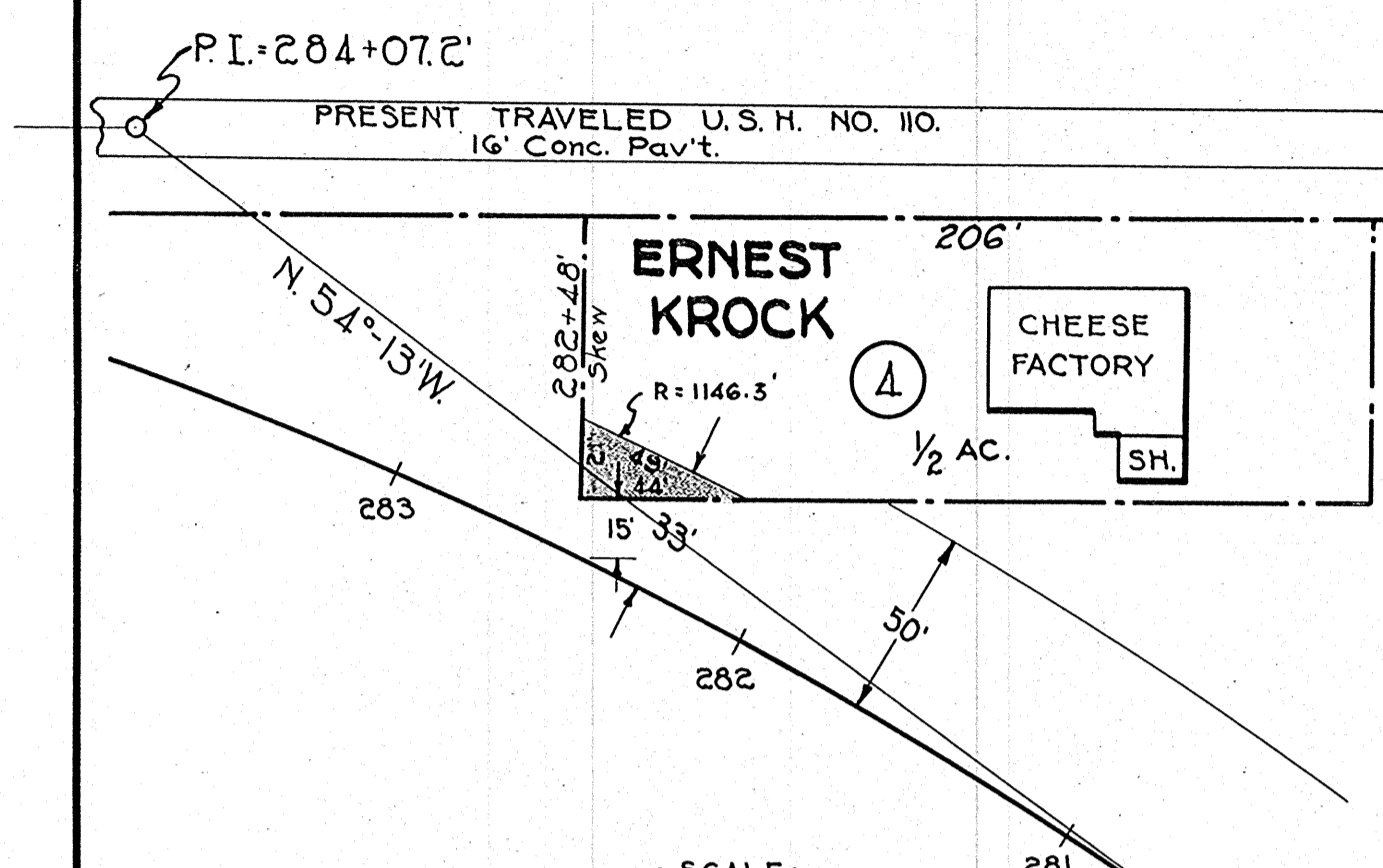
④  
**E. KROCK** (CHEESE FACT)  
 (SEE DETAIL ON LEFT.)

PI: 263+400'  
 $\Delta$ : 6°-00'  
 D: 1°-30'  
 T: 200.2'  
 LC: 400.0'  
 PC: 261+39.8'  
 PT: 265+39.8'

PI: 248+600'  
 $\Delta$ : 1°-56' RT.  
 D: 1°-00'  
 T: 96.7'  
 LC: 193.3'  
 PC: 247+63.3'  
 PT: 249+56.6'

PI: 224+50.3'  
 $\Delta$ : 27°-53'  
 D: 5°-00'  
 T: 284.6'  
 LC: 557.7'  
 PC: 221+65.7'  
 PT: 227+23.4'

PI: 213+62.6'  
 $\Delta$ : 0°-40' LT.

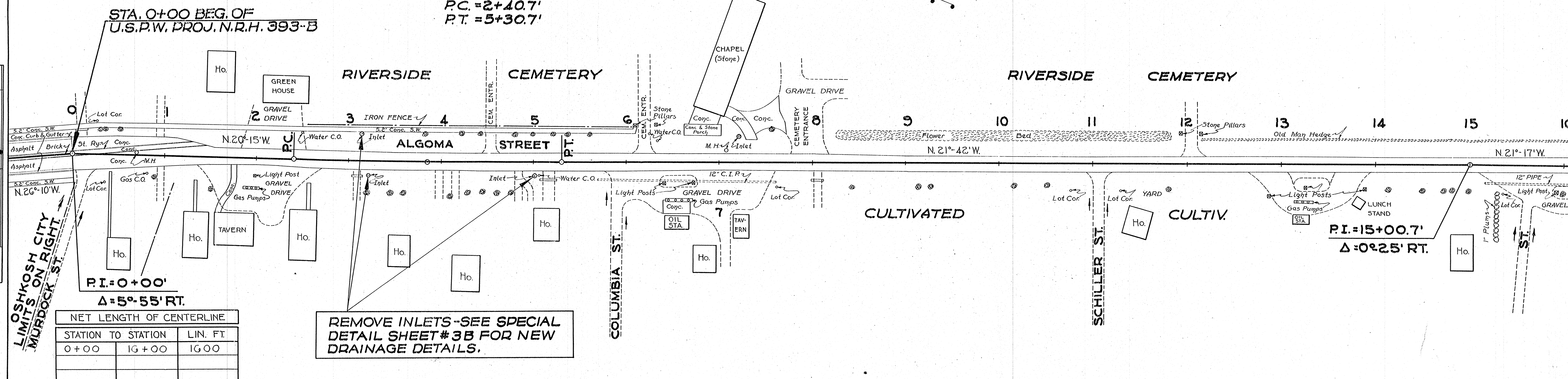
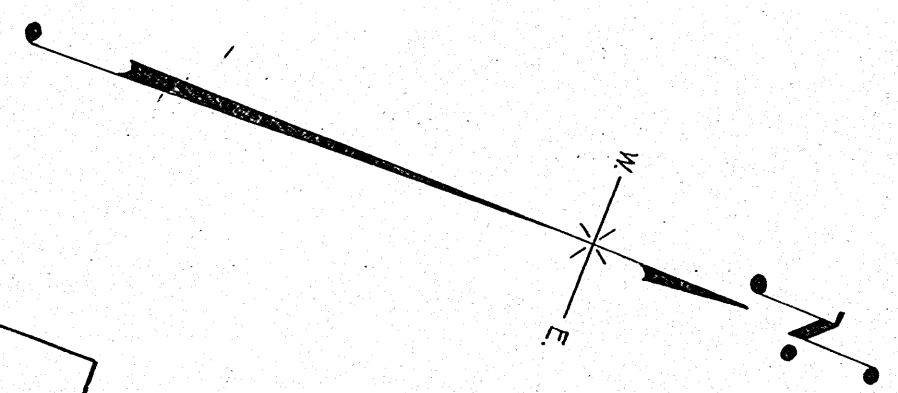


PLAT OF RIGHT OF WAY REQUIRED  
 U. S. PUBLIC WORKS PROJECT NO. N. R. H. 393-B.  
 OSHKOSH-BUTTE DES MORTS ROAD  
 U. S. H. NO. 110. WINNEBAGO COUNTY  
 SCALE  
 0' 400' 800'

### BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
1	0+10'	SPIKE IN 30" MAPLE	31' RT. 100.00
2	3+30'	SPIKE IN 20" MAPLE	52' LT. 101.89

$P.I. = 3+85.7'$   
 $\Delta = 1^{\circ} 27' LT.$   
 $D. = 0^{\circ} 30'$   
 $T. = 145.0'$   
 $L.C. = 290.0'$   
 $P.C. = 2+40.7'$   
 $P.T. = 5+30.7'$

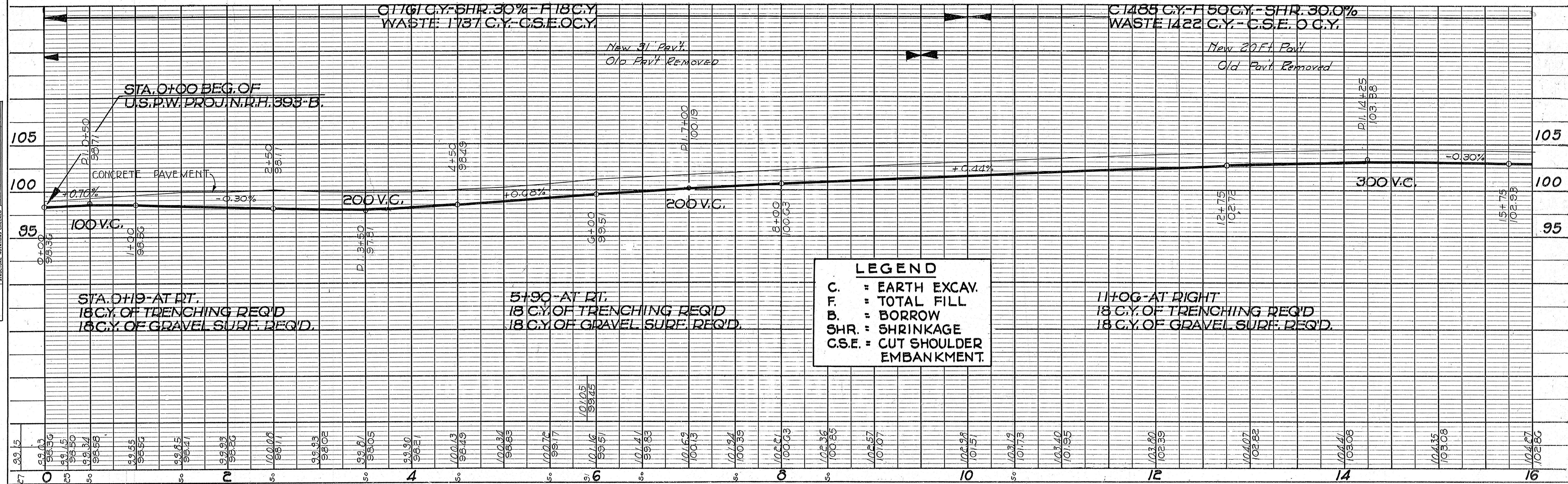


PLAN  
 SURVEYED BY: F.L.V.  
 PLOTTED BY: F.B.H.  
 NOTE BOOK CHECKED: L.V.H.  
 REVISIONS CHECKED: L.V.H.

NET LENGTH OF CENTERLINE

STATION TO STATION	LIN. FT.	
0+00	16+00	1600

REMOVE INLETS - SEE SPECIAL DETAIL SHEET #3B FOR NEW DRAINAGE DETAILS.



**LEGEND**

- C. = EARTH EXCAV.
- F. = TOTAL FILL
- B. = BORROW
- SHR. = SHRINKAGE
- C.S.E. = CUT SHOULDER EMBANKMENT.

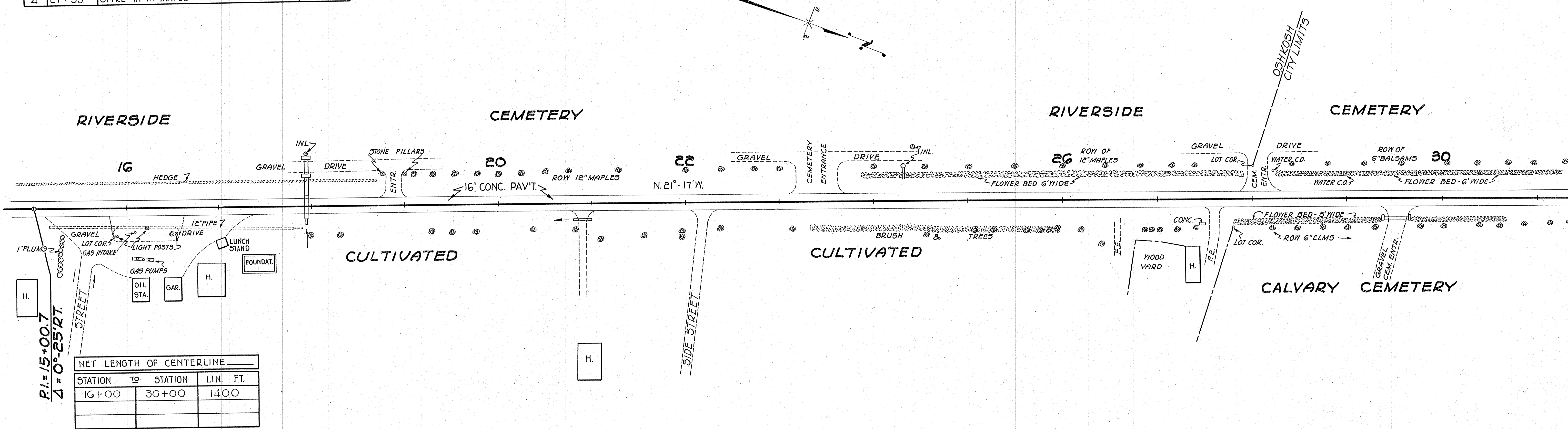
PROFILE  
 SURVEYED BY: F.L.V.  
 PLOTTED BY: F.B.H.  
 NOTE BOOK CHECKED: L.V.H.  
 REVISIONS CHECKED: L.V.H.

**BENCH MARKS**

NO.	STATION	DESCRIPTION	ELEV.
3	18+16	SPIKE IN 10" B. ELDER — 30' LT	104.00
4	27+55	SPIKE IN 14" MAPLE — 55' LT.	108.48

STA. 17+95.5-4'X3.3'X26.6' CONC. CULV. IN PLACE. REMOVE.

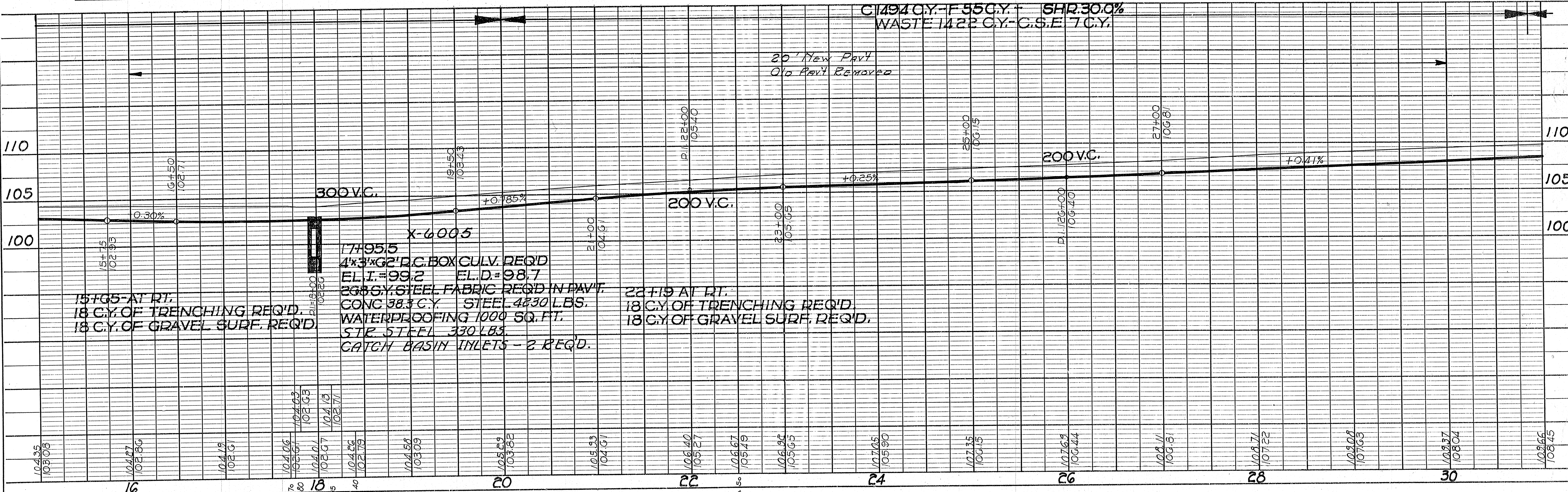
PLAN SURVEYED BY P.L.J. DATE 10-33  
 PLATTED BY F.R.H. DATE 10-33  
 CHECKED BY J.S. DATE 10-33  
 NOTE BOOK NO. 1602-A  
 SHEET NO. 7



NET LENGTH OF CENTERLINE

STATION TO	STATION	LIN. FT.
16+00	30+00	1400

PROFILE SURVEYED BY P.L.J. DATE 10-33  
 PLATTED BY F.R.H. DATE 10-33  
 CHECKED BY J.S. DATE 10-33  
 NOTE BOOK NO. 1602-A  
 SHEET NO. 7



**BENCH MARKS**

NO.	STATION	DESCRIPTION	ELEV.
5	38+85	SPIKE IN 20" ASH	25' RT. 112.34
6	50+80	SPIKE IN 28" OAK	100' RT. 101.70
7	61+20	SPIKE IN 8" B. ELDER	30' RT. 102.62

32+55-3'x2'x26.7' CONC. BOX CULV. IN PLACE. O.K.-EXTEND WITH CONC.

52+09-4'x2'x24' CONC. BOX CULV. IN PLACE. O.K.-EXTEND WITH CONC.

59+95-1-12"x41' C.I.P. CULV. IN PLACE.-REMOVE

54+40-2'x2'x28' CONC. BOX CULV. IN PLACE. O.K.-EXTEND WITH CONC.

60+15-1-20"x41' C.I.P. CULV. IN PLACE.-REMOVE.

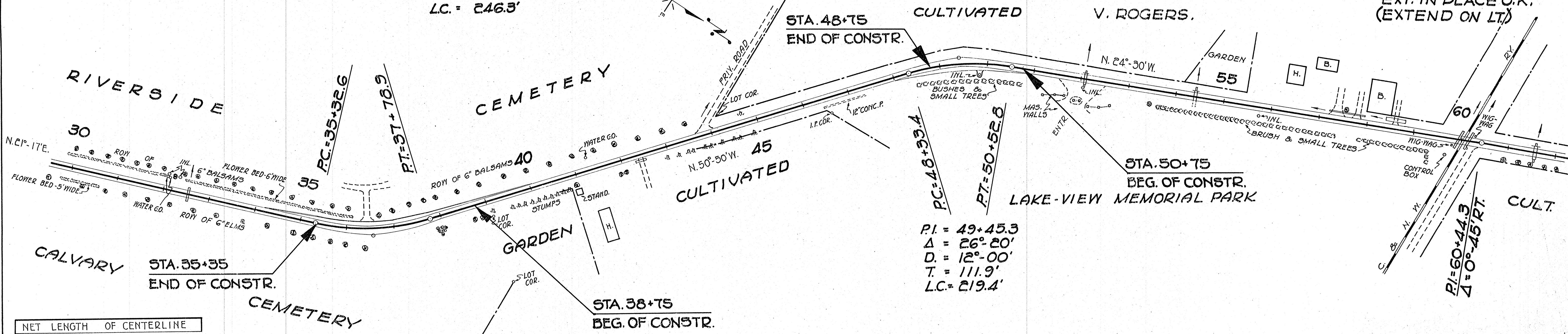
STA. 61+57-2'x2'x26.6' CONC. BOX CULV. WITH 24" C.G.S.M. PIPE EXT. IN PLACE O.K. (EXTEND ON LT.)

PI = 36+58.8  
 $\Delta = 29^{\circ}33'$   
 D = 12^{\circ}00'  
 T = 126.2'  
 LC = 246.3'

PI = 49+45.3  
 $\Delta = 26^{\circ}20'$   
 D = 12^{\circ}00'  
 T = 111.9'  
 LC = 219.4'

PI = 60+44.3  
 $\Delta = 0^{\circ}45' RT.$

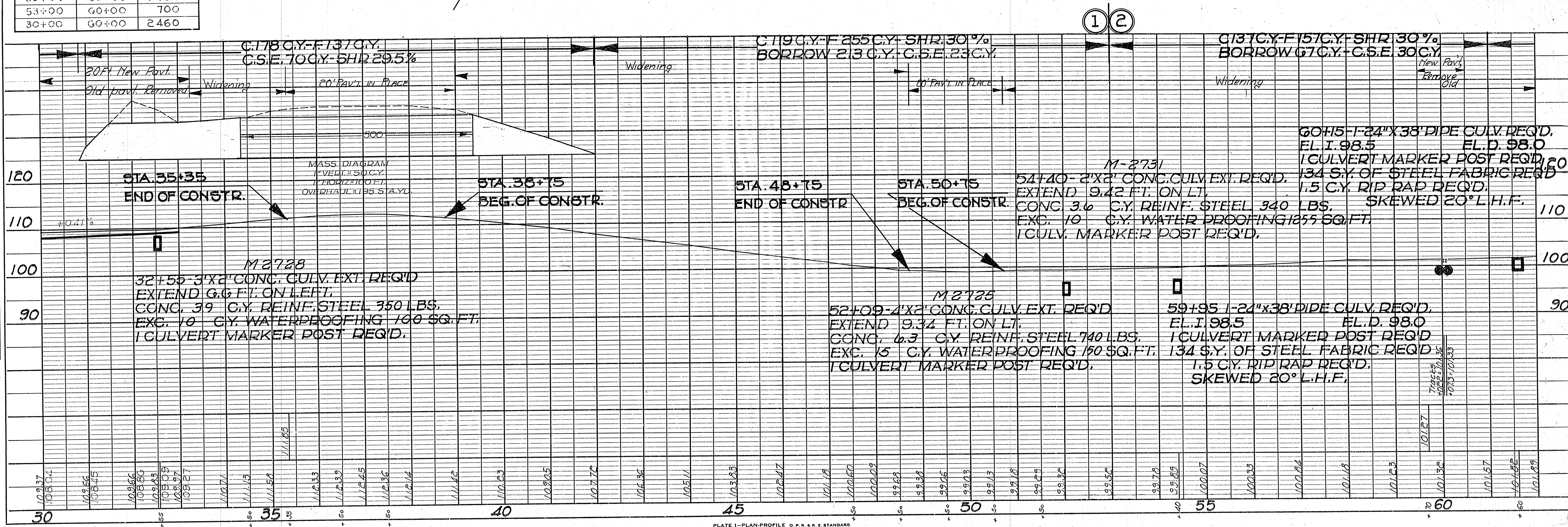
PLAN  
 SURVEYED BY P.L.J.  
 PLOTTED BY H.E.Z.  
 ALIGNED CHECKED BY L.V.E.  
 REVISIONS BY C.C.E.  
 NO. 10305A



NET LENGTH OF CENTERLINE

STATION TO	STATION	LIN. FT.
30+00	53+00	1760
53+00	60+00	700
30+00	60+00	2460

PROFILE  
 SURVEYED BY P.L.J.  
 PLOTTED BY H.E.Z.  
 ALIGNED CHECKED BY L.V.E.  
 REVISIONS BY C.C.E.  
 NO. 10305A





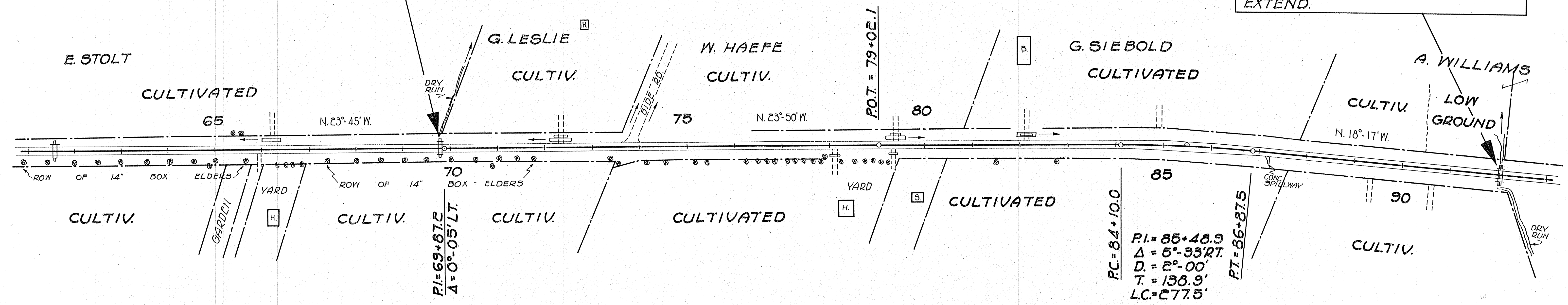
**BENCH MARKS**

NO.	STATION	DESCRIPTION	ELEV.
8	70+95	SPIKE IN 24" OAK - 29' RT.	104.85
9	81+82	SPIKE IN S.E. COR. BARN - 150' LT.	104.17
10	90+33	SPIKE IN P. POLE - 32' RT.	91.70

**69+78-2'X1.5'X26.3' CONC. BOX CULV. WITH 24" CGS.M. PIPE EXT. IN PLACE. (O.K.) EXTEND.**

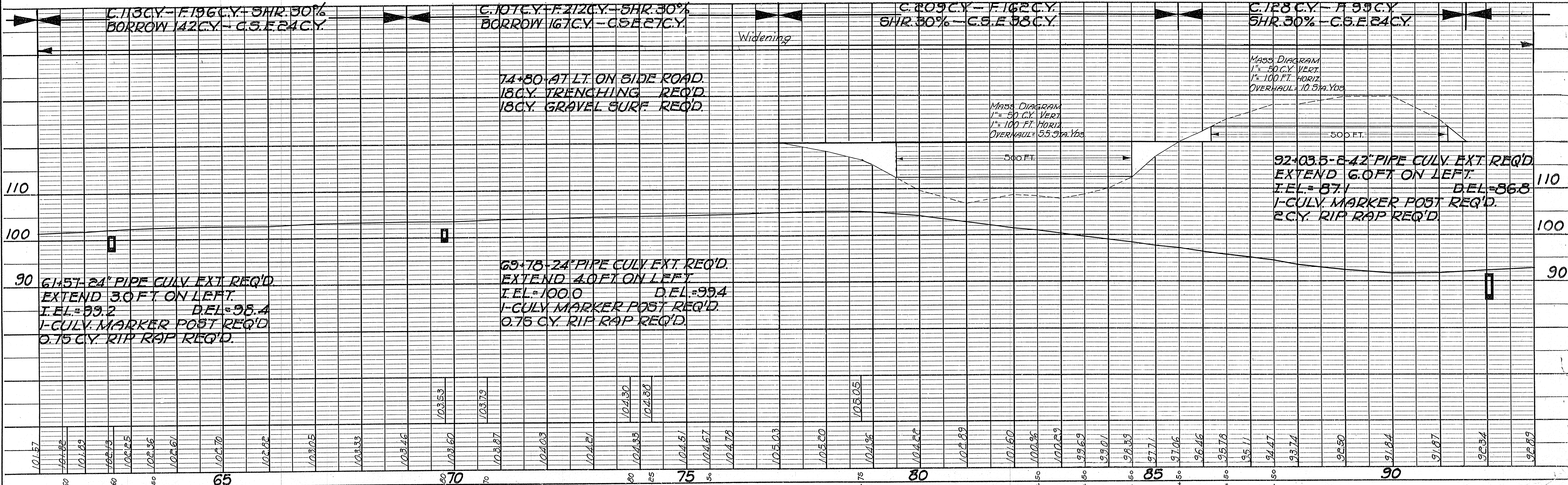
**92+03.5-6'X4'X26.7' CONC. BOX CULV. 2-42" CGS.M. PIPE EXT. IN PLACE. (O.K.) EXTEND.**

PLAN BY P.L.L. 10-33  
 CHECKED BY F.B.H. 10-33  
 NOTE BOOK ALIGNED CHECKED BY L.L.B. 10-33  
 No. 16094



NET LENGTH OF CENTERLINE

STATION TO	STATION FROM	LIN. FT.
60+00	90+00	3000.0



PROFILE BY P.L.L. 10-33  
 CHECKED BY F.B.H. 10-33  
 NOTE BOOK GRADES CHECKED BY L.L.B. 10-33  
 No. 16094

FED. ROAD DIST. NO.	STATE	U.S.P.W. PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	N.E.H. 383-B	10	56

DIVISION JOB NO. 3501

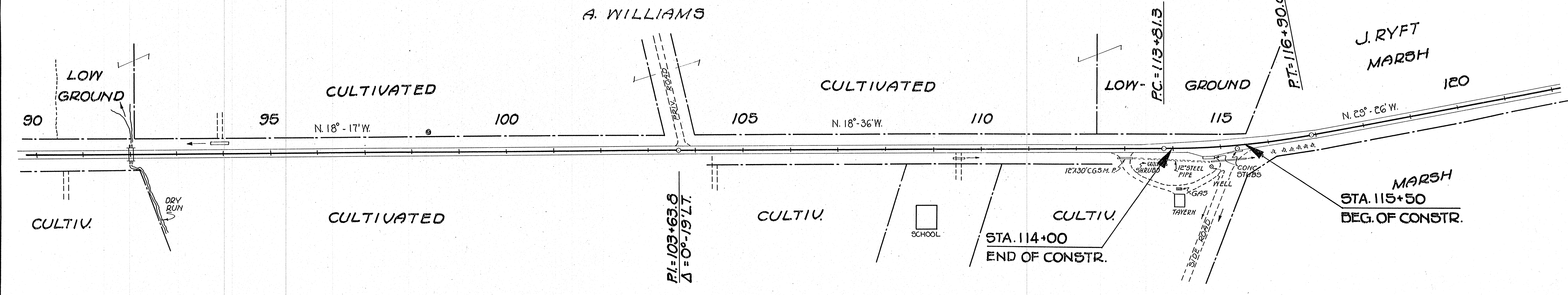
### BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
11	98 + 40	SPIKE IN 30' OAK	96.14
12	108 + 90	TOP N.W. COR. CONC. STEP	97.53
13	114 + 98	SPIKE IN 30' WILLOW	88.03

$PI = 115 + 36.5$   
 $\Delta = 10^{\circ} 30' LT.$   
 $D = 3^{\circ} 30'$   
 $T = 155.2'$   
 $LC = 309.5'$

115+38-18"X68' CGS.M. PIPE CULV. IN PLACE UNDER SIDE RD. ON RT.-O.K.

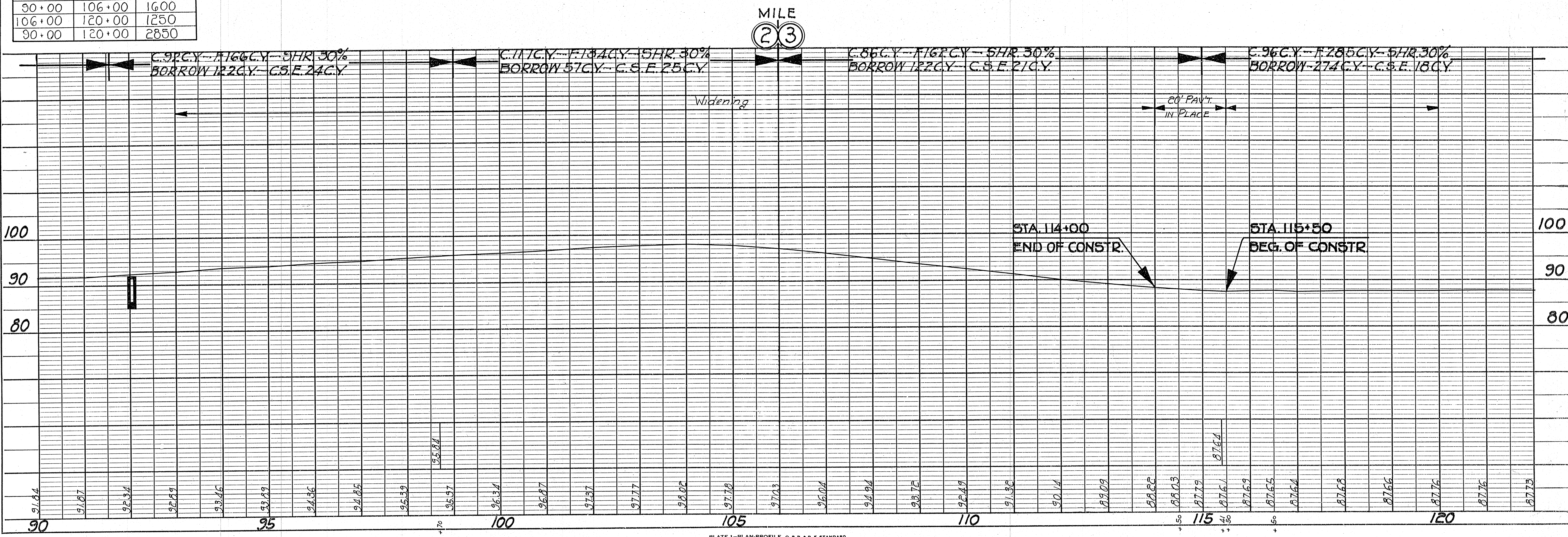
PLAN  
 CHECKED BY: P.L.H.  
 PLOTTED BY: L.V.E.  
 NOTE BOOK GRADE CHECKED: L.V.E.  
 NO. 16025-A



### NET LENGTH OF CENTERLINE

STATION TO	STATION	LIN. FT.
90+00	106+00	1600
106+00	120+00	1250
90+00	120+00	2850

PROFILE  
 CHECKED BY: P.L.H.  
 PLOTTED BY: L.V.E.  
 NOTE BOOK GRADE CHECKED: L.V.E.  
 NO. 16025-A

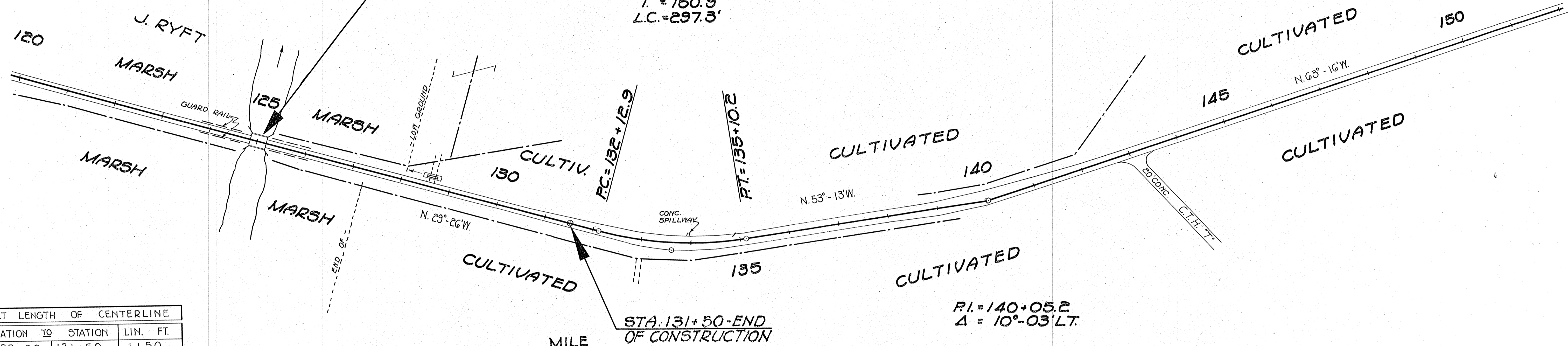
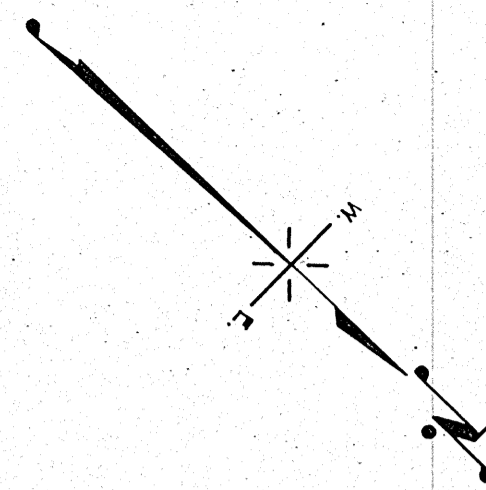


**BENCH MARKS**

NO.	STATION	DESCRIPTION	ELEV.
14	126 + 95	SPIKE IN P. POLE	28' RT. 86.12
15	137 + 75	SPIKE IN 6" APPLE	180' RT. 94.76
16	148 + 28	SPIKE IN 6" APPLE	- LT. 93.92

125+02-30' SPAN, 23.5' R'DWAY  
CONC. BRIDGE IN PLACE - O.K.  
(EXTEND)

PI = 133+63.8  
Δ = 23°-47' LT.  
D = 8°-00'  
T = 150.9'  
L.C. = 297.3'



NET LENGTH OF CENTERLINE

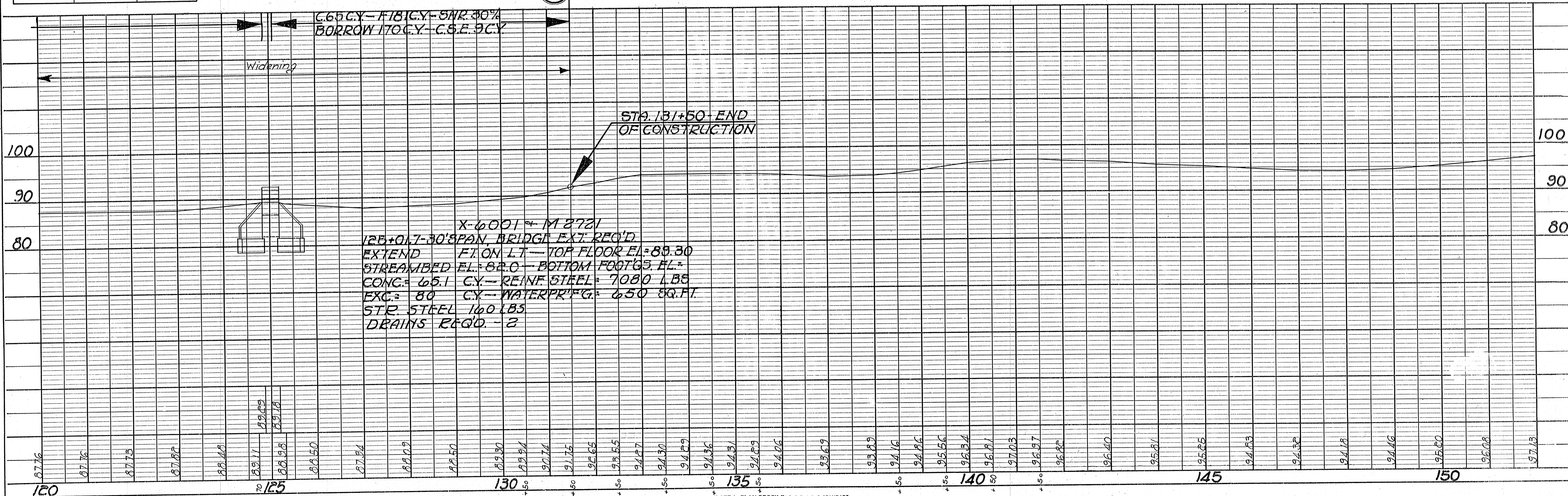
STATION TO	STATION	LIN. FT.
120+00	131+50	1150

PLAN

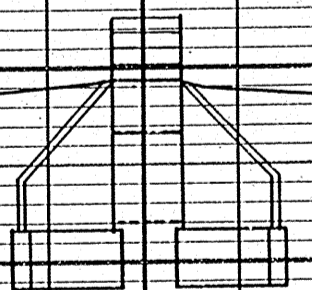
DATE	10-33
BY	P.L.L.
PLANNED	F.B.H.
NOTED	J.D.A.
CHECKED	L.L.B.
NO. 156294	

PROFILE

DATE	10-33
BY	P.L.L.
PLANNED	F.B.H.
NOTED	J.D.A.
CHECKED	L.L.B.
NO. 156294	



6.65 CY - FILL - 5% SNR 30%  
BORROW 170 CY - C.S.E. 9% CY



X-6001 - M 2721  
125+02-30' SPAN, BRIDGE EXT. REQ'D  
EXTEND FT. ON LT. - TOP FLOOR EL. = 89.30  
STREAMBED EL. = 82.0 - BOTTOM FOOTING EL. =  
CONC. = 65.1 CY - REINF. STEEL = 7080 LBS  
EXC. = 80 CY - WATER PUMP = 650 SQ. FT.  
STR. STEEL 160 LBS  
DRAINS REQ'D. - 2

**BENCH MARKS**

NO.	STATION	DESCRIPTION	ELEV.
17	158 + 50	SPIKE IN 8" HICKORY 30' RT	102.99
18	168 + 75	SPIKE IN 12" APPLE 45' RT.	115.51
19	179 + 18	SPIKE IN 10" HICKORY 35' RT.	126.45

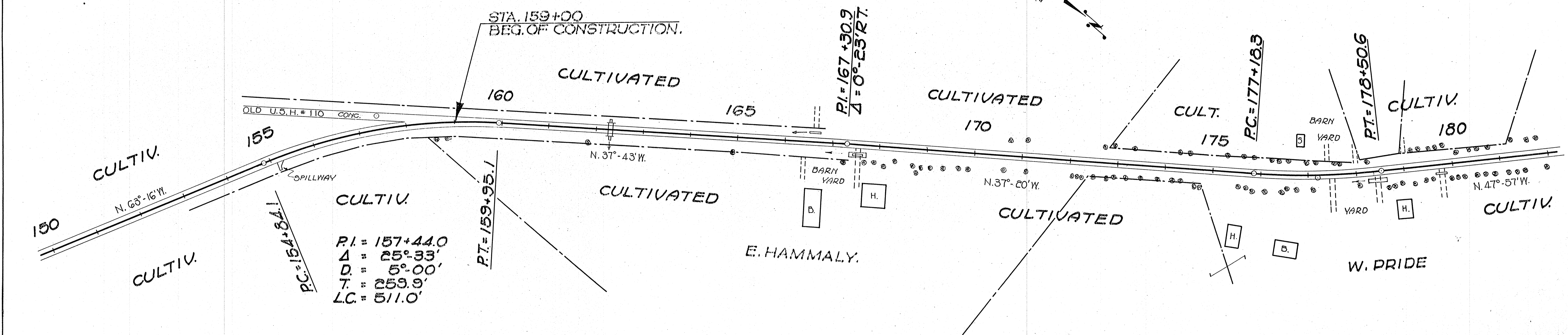
162+29-2'X2'X26' CONC. BOX CULV WITH PIPE EXT'N. IN PLACE. REMOVE.

FED. ROAD DIST. NO.	STATE	U.S.P.W. PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	N.R.H. 393B	12	56

$P.I. = 177+18.3$   
 $\Delta = 10^{\circ}-37'LT.$   
 $D = 4^{\circ}-00'$   
 $T = 133.1'$   
 $L.C. = 265.4'$

DIVISION JOB NO. 3501

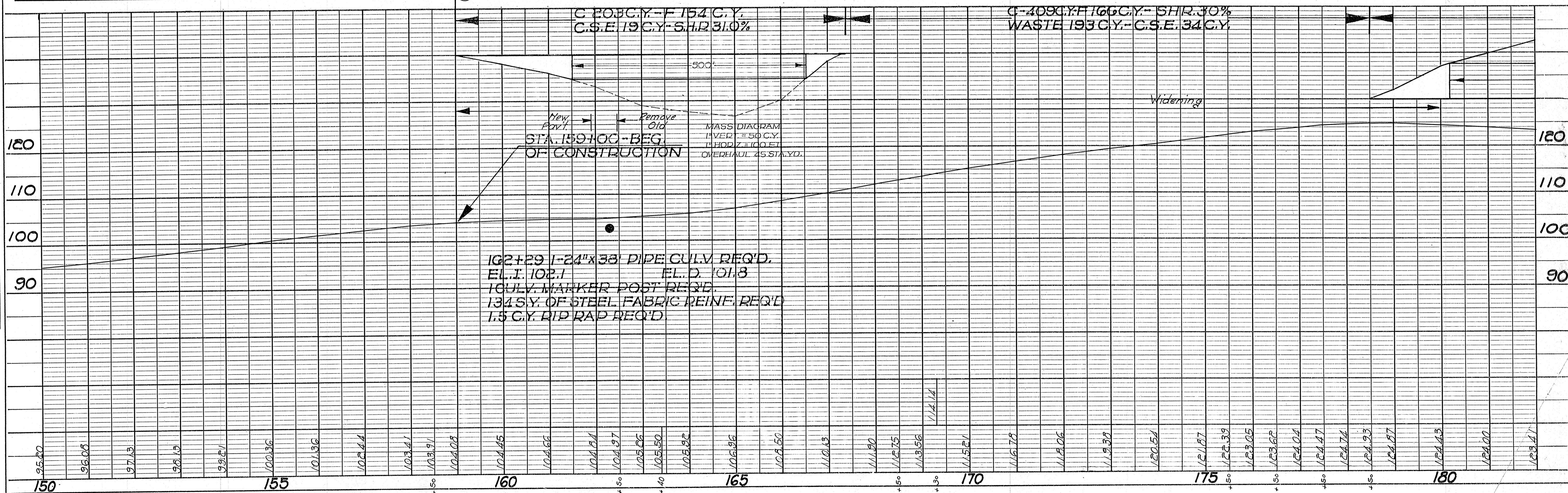
PLAN	DATE	BY
NO. 16007A	10-33	L.L.B.
NOTE BOOK	10-33	L.L.B.
ALIGNED CHECKED	10-33	L.L.B.
RT. OF WAY CHECKED	10-33	L.L.B.



**NET LENGTH OF CENTERLINE**

STATION TO	STATION	LIN. FT.
159+00	180+00	2100

PROFILE	DATE	BY
NO. 16007A	10-33	L.L.B.
NOTE BOOK	10-33	L.L.B.
STRUCTURE NOTATIONS CHECKED	10-33	L.L.B.



**BENCH MARKS**

NO.	STATION	DESCRIPTION	ELEV.
20	188+44'	SPIKE IN 36" ELM	50' RT. 123.19
21	199+75'	SPIKE IN 10" HICKORY	28' RT. 127.77

191+82'-4'x3'x26' CONC. BOX CULV.  
WITH 42"CG.S.M. PIPE EXT'S.-IN PLACE  
O.K. EXTEND

202+15'-2'x1.5'x26' CONC. BOX CULV.  
WITH 24"CG.S.M. PIPE EXT'S.-IN PLACE  
O.K. EXTEND.

FED. ROAD DIST. NO.	STATE	U.S.P.W. PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	393-B	13	56

DIVISION JOB NO. 3501

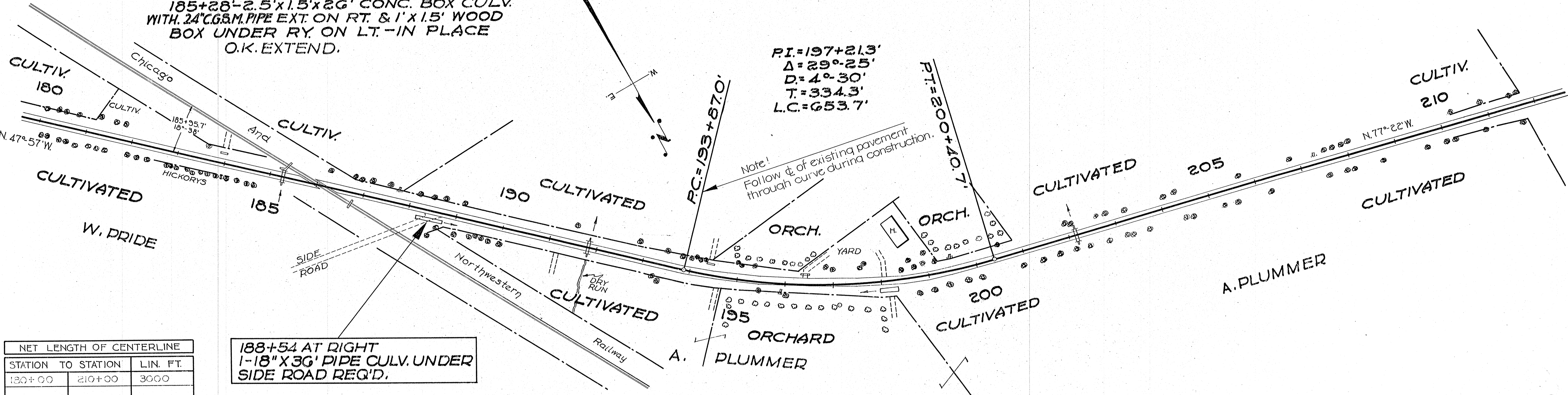
185+28'-2.5'x1.5'x26' CONC. BOX CULV.  
WITH 24"CG.S.M. PIPE EXT. ON RT. & 1'x1.5' WOOD  
BOX UNDER RY. ON LT.-IN PLACE  
O.K. EXTEND.

PI=197+21.3'  
Δ=29°-25'  
D=4°-30'  
T=334.3'  
L.C.=653.7'

RC=193+87.0'  
PT=200+40.7'

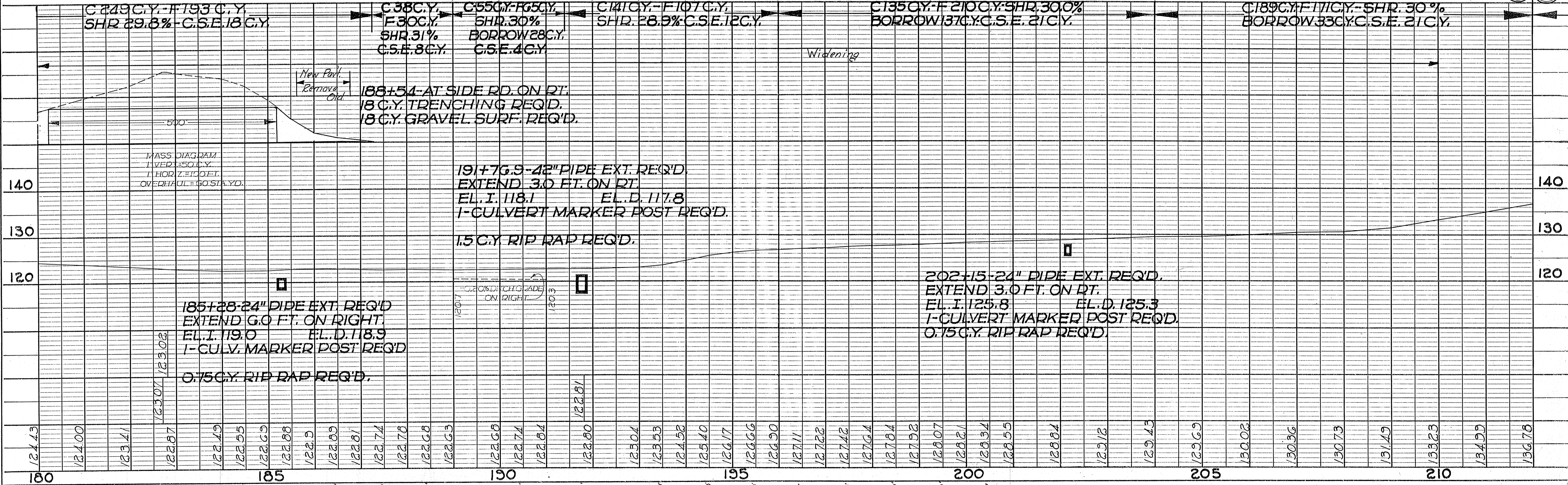
Note!  
Follow Ⓢ of existing pavement  
through curve during construction.

188+54 AT RIGHT  
1-18" X 36" PIPE CULV. UNDER  
SIDE ROAD REQ'D.



NET LENGTH OF CENTERLINE

STATION TO STATION	LIN. FT.	
180+00	210+00	3000



PLAN

BY	DATE
PLJ	10-33
ERH	11-33
WV	12-33
CC	1-34

REVISIONS

NO.	DESCRIPTION
1	AS NOTED
2	AS NOTED

NOTE BOOK CHANGE CHECKED BY: W.C. B. M. 10/33

PROFILE

BY	DATE
PLJ	10-33
ERH	11-33
WV	12-33
CC	1-34

REVISIONS

NO.	DESCRIPTION
1	AS NOTED
2	AS NOTED

NOTE BOOK CHANGE CHECKED BY: W.C. B. M. 10/33

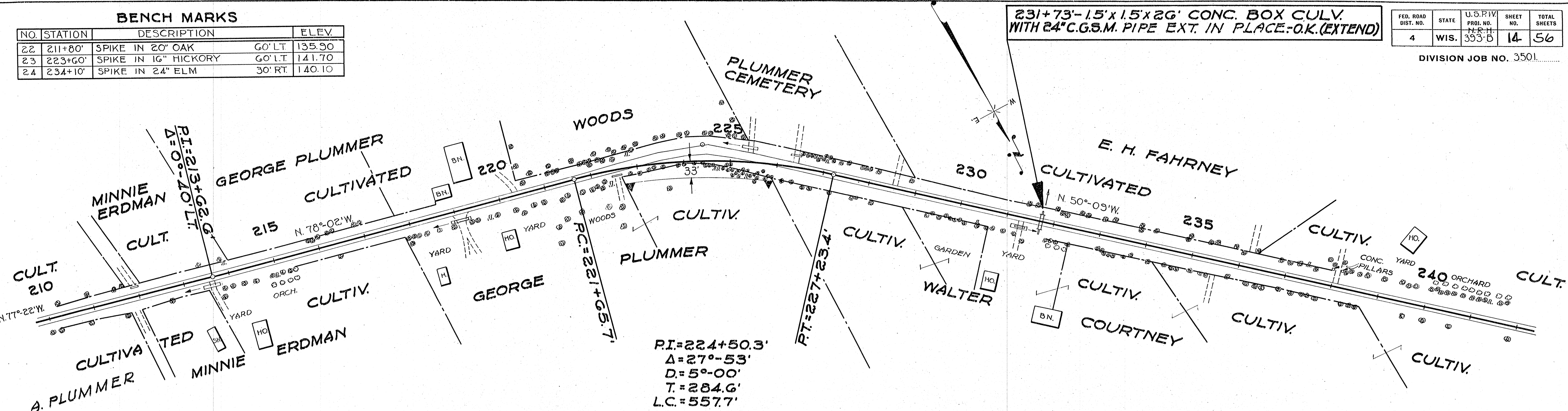
4 5

**BENCH MARKS**

NO.	STATION	DESCRIPTION	ELEV.
22	211+80'	SPIKE IN 20" OAK	60' LT 135.90
23	223+60'	SPIKE IN 16" HICKORY	60' LT 141.70
24	234+10'	SPIKE IN 24" ELM	30' RT. 140.10

FED. ROAD DIST. NO.	STATE	U.S.P.I.V. PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	333-B	14	56

DIVISION JOB NO. 3501

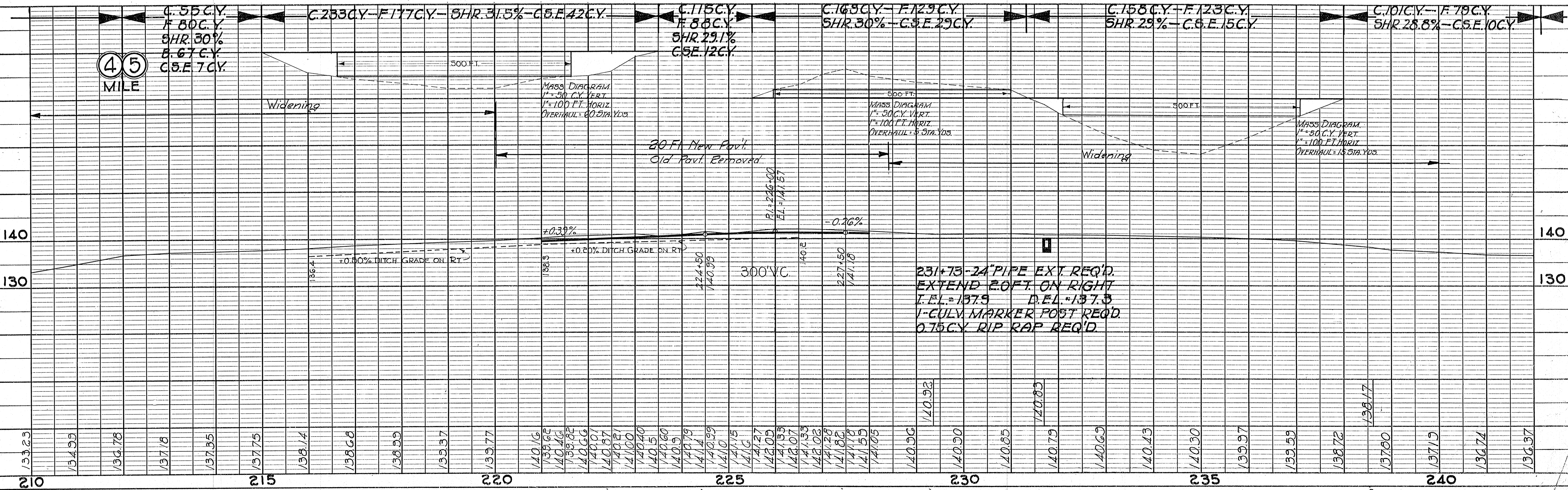


PI=224+50.3'  
 Δ=27°-53'  
 D=5°-00'  
 T=284.6'  
 L.C.=557.7'

**NET LENGTH OF CENTERLINE**

STATION TO STATION	LIN. FT.
210+00 212+00	200
212+00 240+00	2800
210+00 240+00	3000

45 MILE



231+73-24" PIPE EXT. REQ'D.  
 EXTEND 20 FT. ON RIGHT  
 I.E.L.=137.9 D.E.L.=137.9  
 1'-CULV. MARKER POST REQ'D.  
 0.75 CY RIP RAP REQ'D.

PLANNED BY: P.L.J., H.E.Z., J.D.L., R.F.C.  
 CHECKED BY: P.L.J., H.E.Z., J.D.L., R.F.C.  
 DATE: 11-33-33  
 NO. 7610-A

PROFILE BY: P.L.J., H.E.Z., J.D.L., R.F.C.  
 CHECKED BY: P.L.J., H.E.Z., J.D.L., R.F.C.  
 DATE: 11-33-33  
 NO. 7610-A

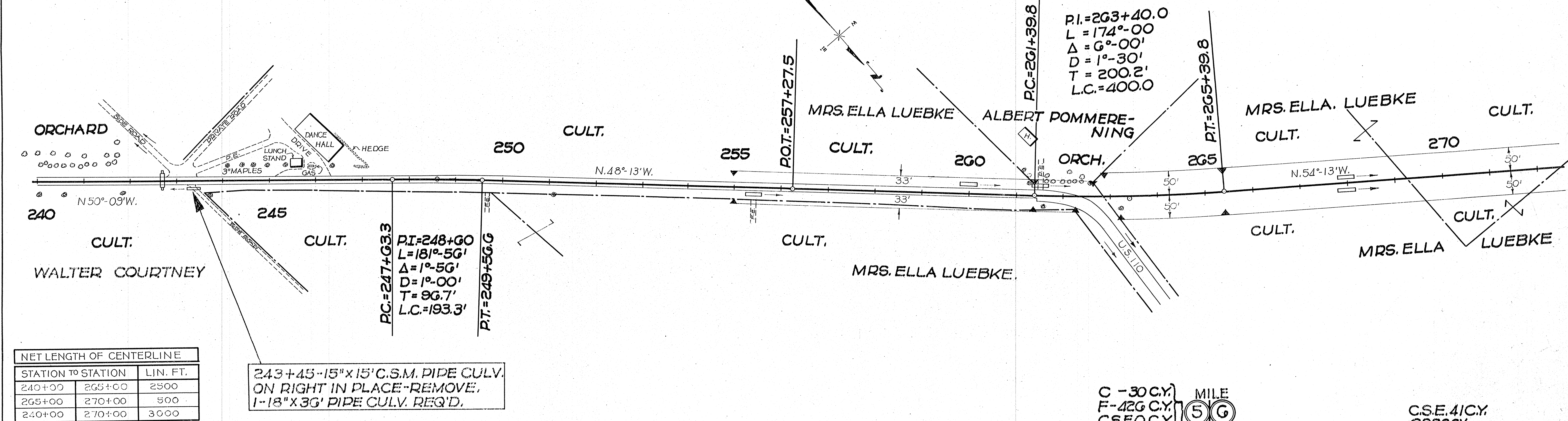
**BENCH MARKS**

N <sup>o</sup>	STATION	DESCRIPTION	ELEV.
25	246+26	SPIKE IN COR. DANCE HALL	34'LT. 136.11
26	261+09	SPIKE IN G <sup>o</sup> HICKORY	60'LT. 136.20

242+71-1.5'x1.5'x26.4' CONC. BOX CULV. WITH 24" C.G.S.M. PIPE EXT. IN PLACE. O.K. EXTEND.

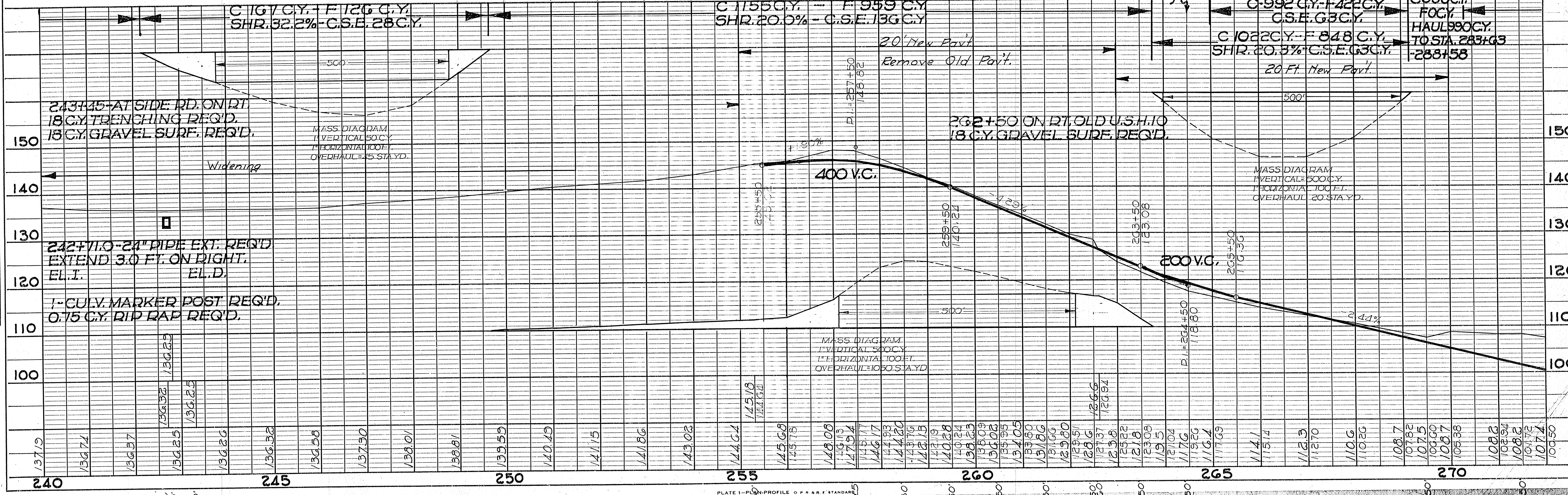
BY	PL	DATE
BY	PL	DATE
BY	PL	DATE

BY	PL	DATE
BY	PL	DATE
BY	PL	DATE



NET LENGTH OF CENTERLINE

STATION TO STATION	LIN. FT.
240+00 265+00	2500
265+00 270+00	500
240+00 270+00	3000



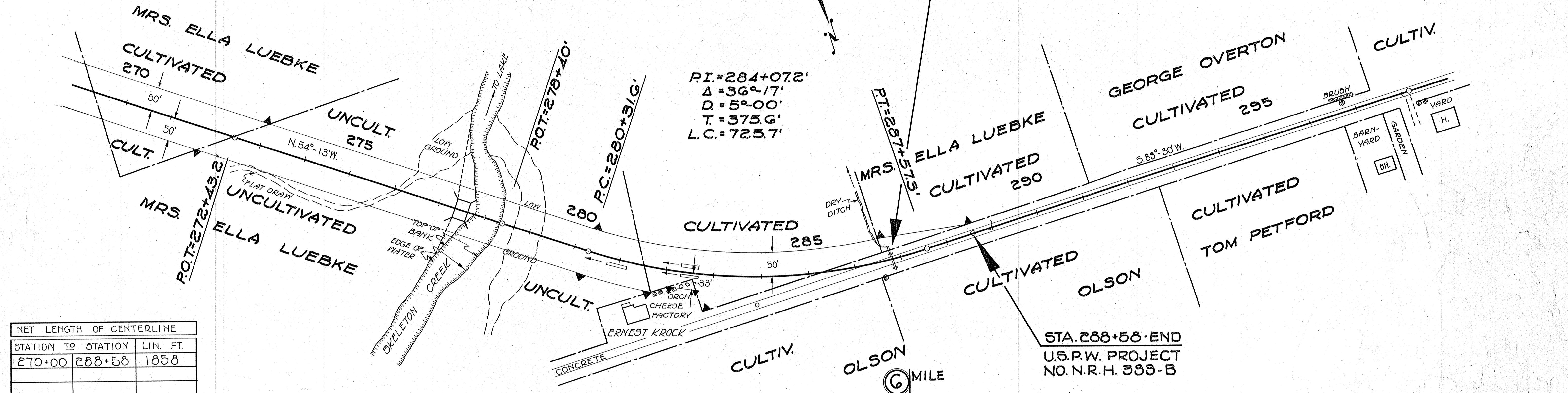
**BENCH MARKS**

NO.	STATION	DESCRIPTION	ELEV.
27	272 + 28	5PIKE IN 16' OAK - 44' RT.	104.38
28	277 + 14	5PIKE IN 16' TWIN ASH - 110' RT.	88.07
29	280 + 30	5PIKE IN 20' POPLAR - 270' RT.	93.22
30	285 + 22	5PIKE IN 20' OAK - 120' RT.	103.62

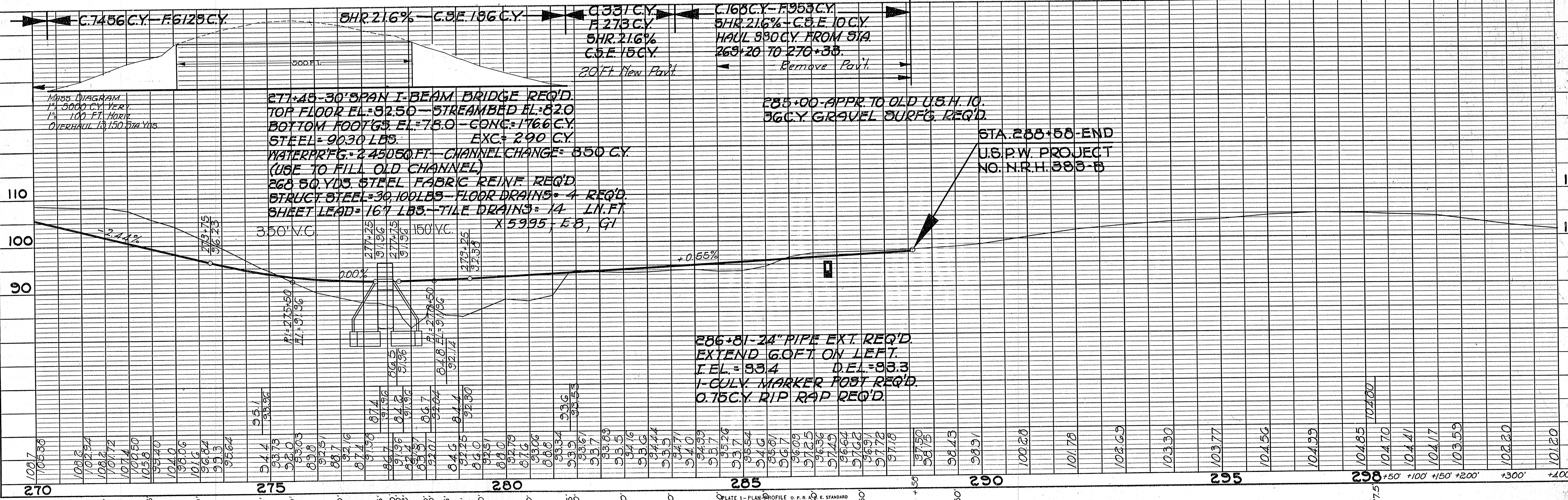
FED. ROAD DIST. NO.	STATE	U.S.P.W. PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	N.R.H. 333-B	16	56

DIVISION JOB NO. 3501

PLAN	DATE	BY
SURVEYED	10-23-33	H. V. F.
NOTED	11-3-33	C. C. E.
PLANNED		
NOTED		
PLANNED		
NOTED		
PLANNED		
NOTED		



PROFILE	DATE	BY
SURVEYED	10-23-33	H. V. F.
NOTED	11-3-33	C. C. E.
PLANNED		
NOTED		
PLANNED		
NOTED		
PLANNED		
NOTED		



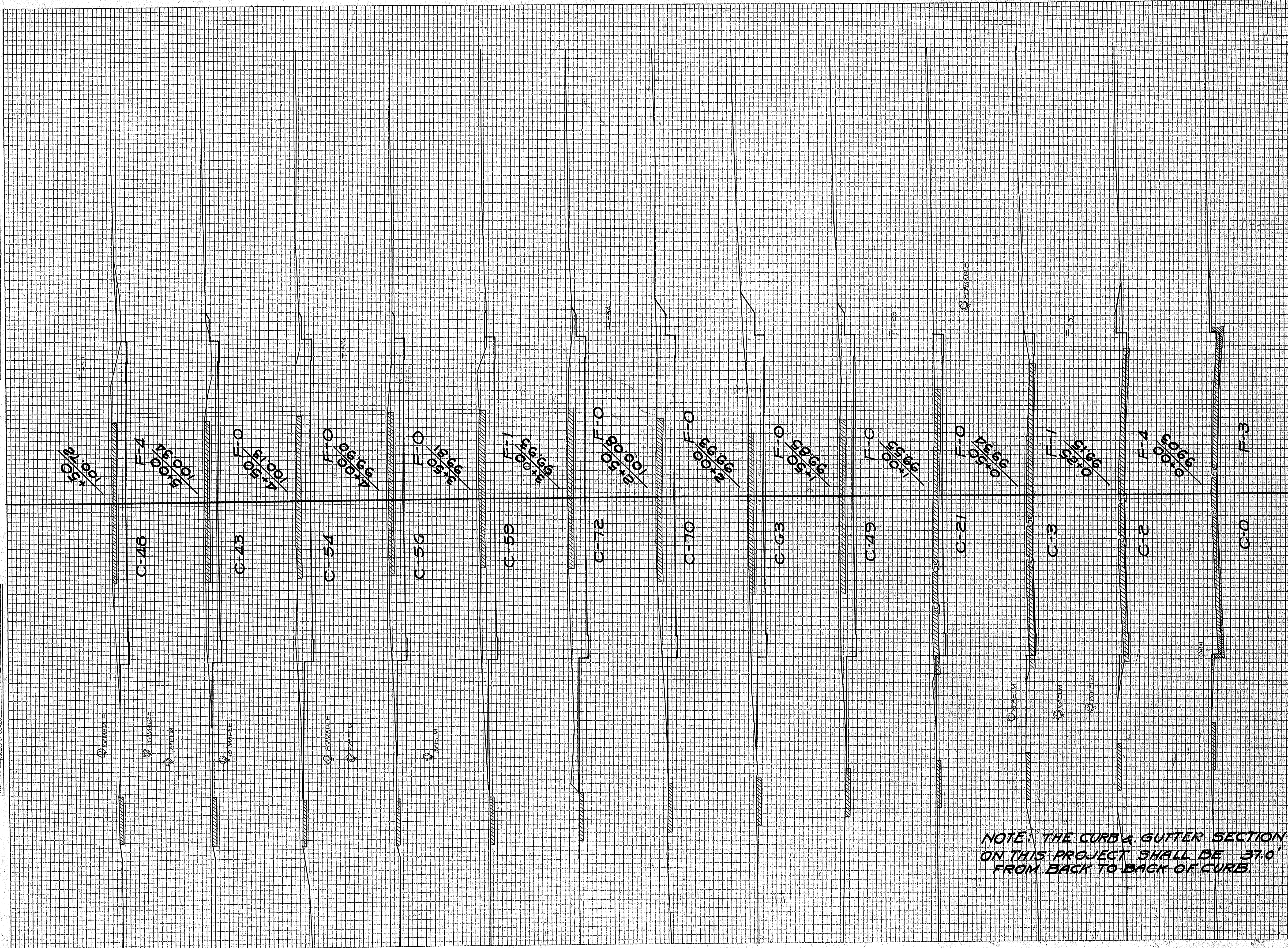


CULVERT SCHEDULE FOR EXTENSIONS <span style="float: right;">○ SHEET ○</span>																					
NAME OF ROAD <u>OSHKOSH - BUTTE DES MORTS</u> COUNTY <u>WINNEBAGO</u> U.S.P.W. PROJ. NO. <u>N.R.H. 393-B</u> DIV. JOB NO. <u>3501</u>																					
CONCRETE CULVERTS										PIPE CULVERTS											
MILE NO.	PLAN SHEET NO.	STA-TION	SIZE			CONC-RETE	STEEL	EXCAV-ATION	WATER-PROOFING	REMARKS <small>ON EXTENSIONS STATE LENGTH ADDED ON EACH END OF PRESENT CULV. ALSO STATE TYPE AND SLOPE OF END WALLS</small>	MILE NO.	PLAN SHEET NO.	STA-TION	DIA-METER	LENGTH	TYPE <small>C.G.S.M. CAST I. CONC.</small>	CLASS OF SHEET METAL IF ANY	LOCATION			REMARKS
			WIDTH	HEIGHT	LENGTH													UNDER ROAD-WAY	SIDE ROAD OR PR. ENT	RIGHT LEFT	
1	8	32+55	3'	2'	267	✓				6.6 ft. on Lt. (Conc.)											
1	8	52+09	4'	2'	240	✓				9.34 ft. on Lt. (Conc.)											
2	8	54+40	2'	2'		✓				9.42 ft. on Lt. (Conc.)											
											2	8	61+57	24"	30'	C.G.S.M.		✓			1-24" C.G.S.M. PIPE EXT. ON LT.
											2	9	69+78	24"	40'	C.G.S.M.		✓			1-24" C.G.S.M. PIPE EXT. ON LT.
											2	9	72+03.5	42"	60'	C.G.S.M.		✓			2-42"x6" C.G.S.M. PIPE EXT. ON LT.
											4	13	185+28	24"	60'	C.G.S.M.		✓			1-24" C.G.S.M. PIPE EXT. ON RT.
											4	13	191+82	42"	30'	C.G.S.M.		✓			1-42" C.G.S.M. PIPE EXT. ON RT.
											4	13	202+15	24"	30'	C.G.S.M.		✓			1-24" C.G.S.M. PIPE EXT. ON RT.
											5	14	231+73	24"	20'	C.G.S.M.		✓			1-24" C.G.S.M. PIPE EXT. ON RT.
											5	15	242+71	24"	30'	C.G.S.M.		✓			1-24" C.G.S.M. PIPE EXT. ON RT.
											6	16	286+81.1	24"	60'	C.G.S.M.		✓			1-24" C.G.S.M. PIPE EXT. ON LT.

NOTE: LIST EACH INDIVIDUAL CULVERT SEPARATELY.

FINAL SURVEY PLOTTED BY DATE  
 SURVEY PLOTTED BY DATE  
 NOTE BOOK NO. AREAS CHECKED.

ORIGINAL SURVEY PLOTTED BY DATE  
 SURVEY PLOTTED BY DATE  
 NOTE BOOK NO. AREAS CHECKED.



NOTE: THE CURB & GUTTER SECTION ON THIS PROJECT SHALL BE 37.0' FROM BACK TO BACK OF CURB.

STATION	END AREA		DISTANCE			YARDAGE			TOTAL
	EARTH CUT	ROCK CUT	EARTH	ROCK	FILL	EARTH CUT	ROCK CUT	TOTAL	
150	0	0	125	1	0	125	0	0	125
145	0	0	125	2	0	125	0	0	125
140	0	0	125	3	0	125	0	0	125
135	0	0	125	4	0	125	0	0	125
130	0	0	125	5	0	125	0	0	125
125	0	0	125	6	0	125	0	0	125
120	0	0	125	7	0	125	0	0	125
115	0	0	125	8	0	125	0	0	125
110	0	0	125	9	0	125	0	0	125
105	0	0	125	10	0	125	0	0	125
100	0	0	125	11	0	125	0	0	125
95	0	0	125	12	0	125	0	0	125
90	0	0	125	13	0	125	0	0	125
85	0	0	125	14	0	125	0	0	125
80	0	0	125	15	0	125	0	0	125
75	0	0	125	16	0	125	0	0	125
70	0	0	125	17	0	125	0	0	125
65	0	0	125	18	0	125	0	0	125
60	0	0	125	19	0	125	0	0	125
55	0	0	125	20	0	125	0	0	125
50	0	0	125	21	0	125	0	0	125
45	0	0	125	22	0	125	0	0	125
40	0	0	125	23	0	125	0	0	125
35	0	0	125	24	0	125	0	0	125
30	0	0	125	25	0	125	0	0	125
25	0	0	125	26	0	125	0	0	125
20	0	0	125	27	0	125	0	0	125
15	0	0	125	28	0	125	0	0	125
10	0	0	125	29	0	125	0	0	125
5	0	0	125	30	0	125	0	0	125
0	0	0	125	31	0	125	0	0	125
TOTAL	0	0	125	32	0	125	0	0	125



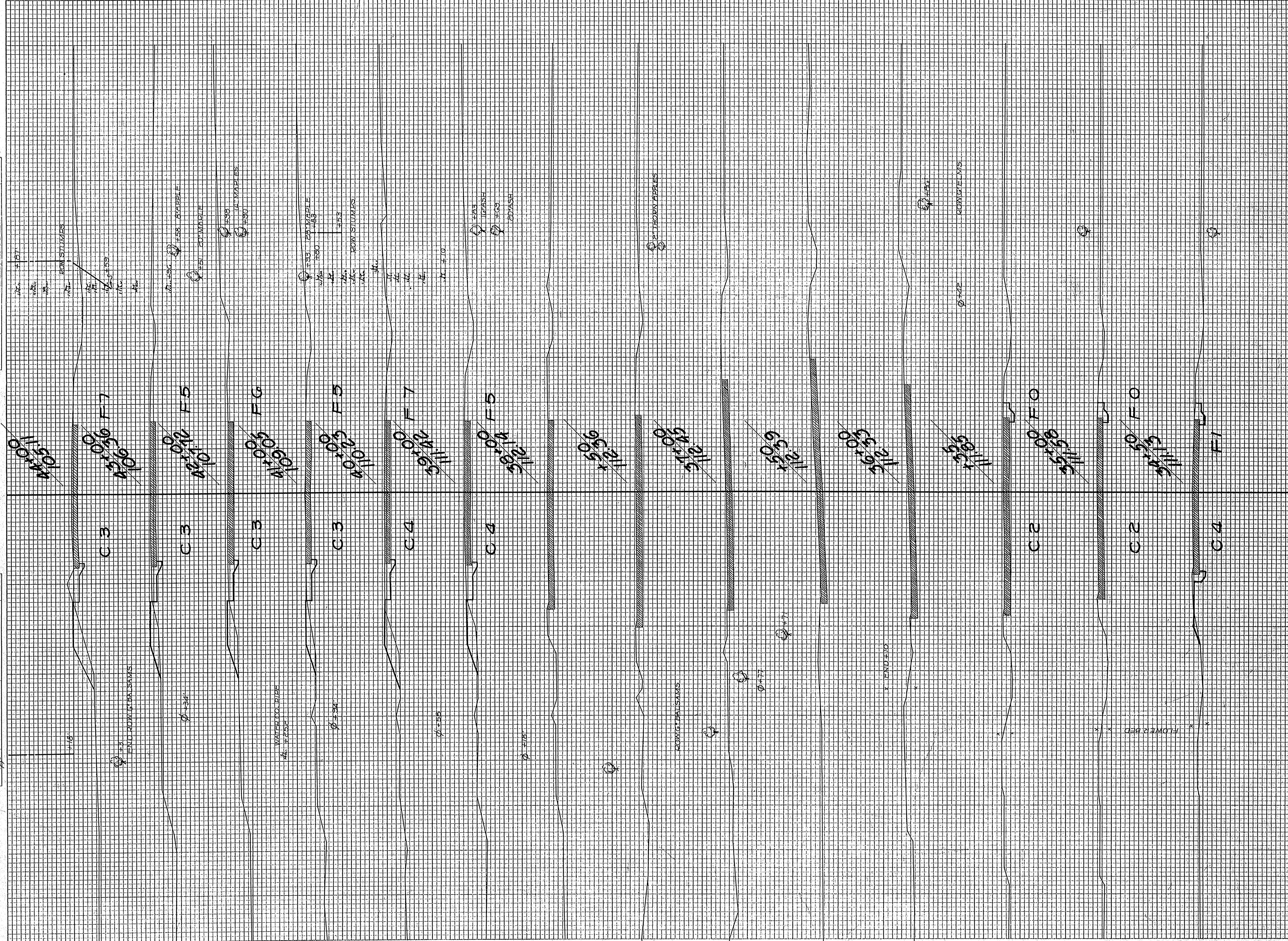




FINAL SURVEYED BY DATE  
 SURVEY PLOTTED BY  
 NOTE BOOK TEMPLATE NO.  
 AREAS CHECKED

ORIGINAL SURVEYED BY DATE  
 SURVEY PLOTTED BY  
 NOTE BOOK TEMPLATE NO.  
 AREAS CHECKED

U.S.P.O. SHEET NO. 28 TOTAL SHEETS 36  
 STATE WISC. PROJ. NO. 3501  
 DIVISION JOB NO. 3501

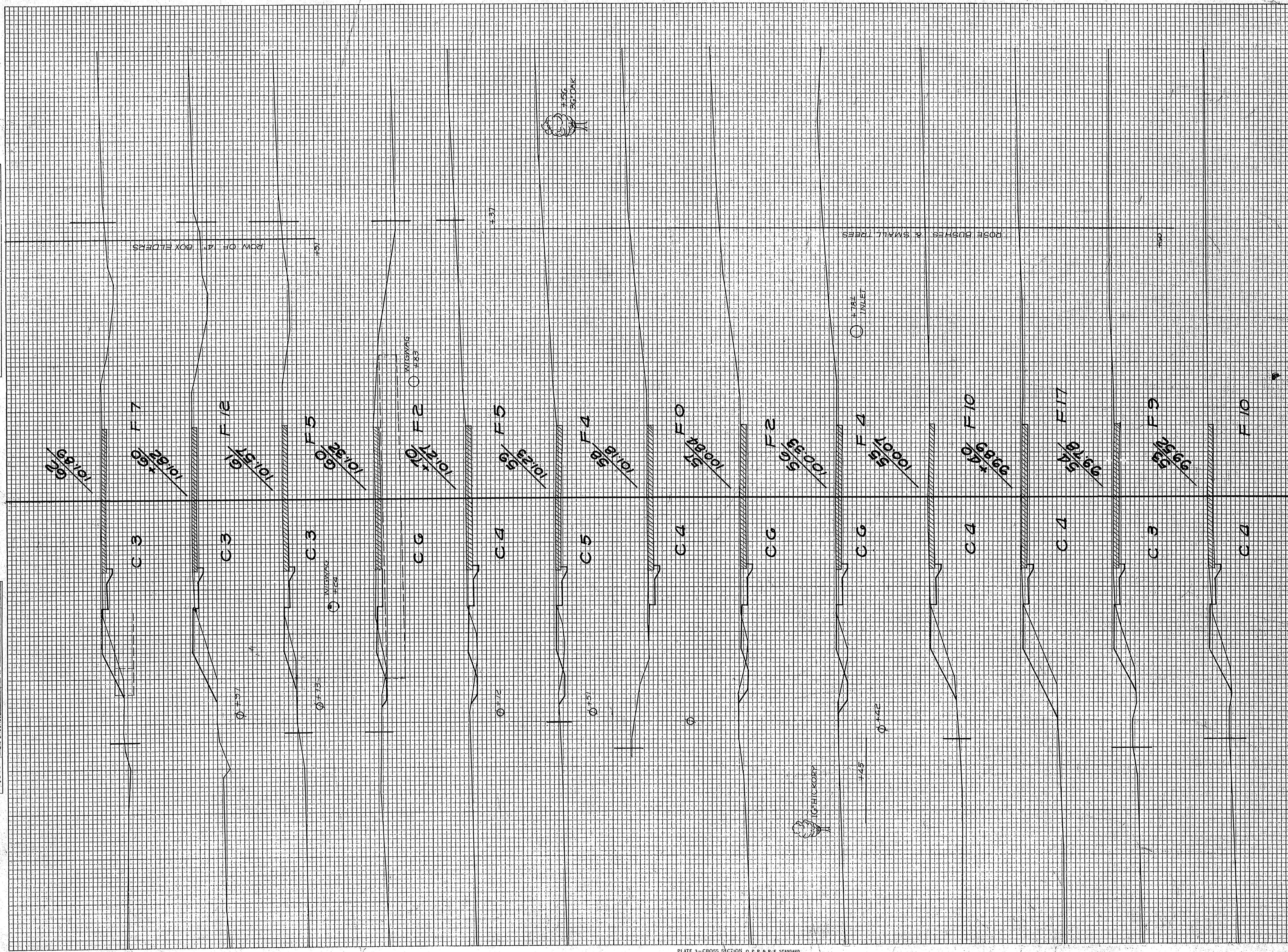


STATION	END AREA			DISTANCE	YARDAGE		
	EARTH CUT	ROCK CUT	FILL		EARTH CUT	ROCK CUT	TOTAL FILL
34	4	0	0	0	0	0	0
35	4	0	0	0	0	0	0
36	2	0	0	0	0	0	0
37	2	0	0	0	0	0	0
38	0	0	0	0	0	0	0
39	0	0	0	0	0	0	0
40	4	0	0	0	0	0	0
41	4	0	0	0	0	0	0
42	3	0	0	0	0	0	0
43	3	0	0	0	0	0	0
44	3	0	0	0	0	0	0
45	7	0	0	0	0	0	0
46	11	0	0	0	0	0	0
47	11	0	0	0	0	0	0
48	11	0	0	0	0	0	0
49	11	0	0	0	0	0	0
50	11	0	0	0	0	0	0
51	11	0	0	0	0	0	0
52	11	0	0	0	0	0	0
53	11	0	0	0	0	0	0
54	11	0	0	0	0	0	0
55	11	0	0	0	0	0	0
56	11	0	0	0	0	0	0
57	11	0	0	0	0	0	0
58	11	0	0	0	0	0	0
59	11	0	0	0	0	0	0
60	11	0	0	0	0	0	0
61	11	0	0	0	0	0	0
62	11	0	0	0	0	0	0
63	11	0	0	0	0	0	0
64	11	0	0	0	0	0	0
65	11	0	0	0	0	0	0
66	11	0	0	0	0	0	0
67	11	0	0	0	0	0	0
68	11	0	0	0	0	0	0
69	11	0	0	0	0	0	0
70	11	0	0	0	0	0	0
71	11	0	0	0	0	0	0
72	11	0	0	0	0	0	0
73	11	0	0	0	0	0	0
74	11	0	0	0	0	0	0
75	11	0	0	0	0	0	0
76	11	0	0	0	0	0	0
77	11	0	0	0	0	0	0
78	11	0	0	0	0	0	0
79	11	0	0	0	0	0	0
80	11	0	0	0	0	0	0
81	11	0	0	0	0	0	0
82	11	0	0	0	0	0	0
83	11	0	0	0	0	0	0
84	11	0	0	0	0	0	0
85	11	0	0	0	0	0	0
86	11	0	0	0	0	0	0
87	11	0	0	0	0	0	0
88	11	0	0	0	0	0	0
89	11	0	0	0	0	0	0
90	11	0	0	0	0	0	0
91	11	0	0	0	0	0	0
92	11	0	0	0	0	0	0
93	11	0	0	0	0	0	0
94	11	0	0	0	0	0	0
95	11	0	0	0	0	0	0
96	11	0	0	0	0	0	0
97	11	0	0	0	0	0	0
98	11	0	0	0	0	0	0
99	11	0	0	0	0	0	0
100	11	0	0	0	0	0	0
TOTAL	85	0	0	0	0	0	0
TOTAL	119	0	0	0	0	0	0



FINAL SURVEY PLOTTED. NOTE BOOK AREAS CHECKED.

ORIGINAL SURVEY PLOTTED. NOTE BOOK AREAS CHECKED.





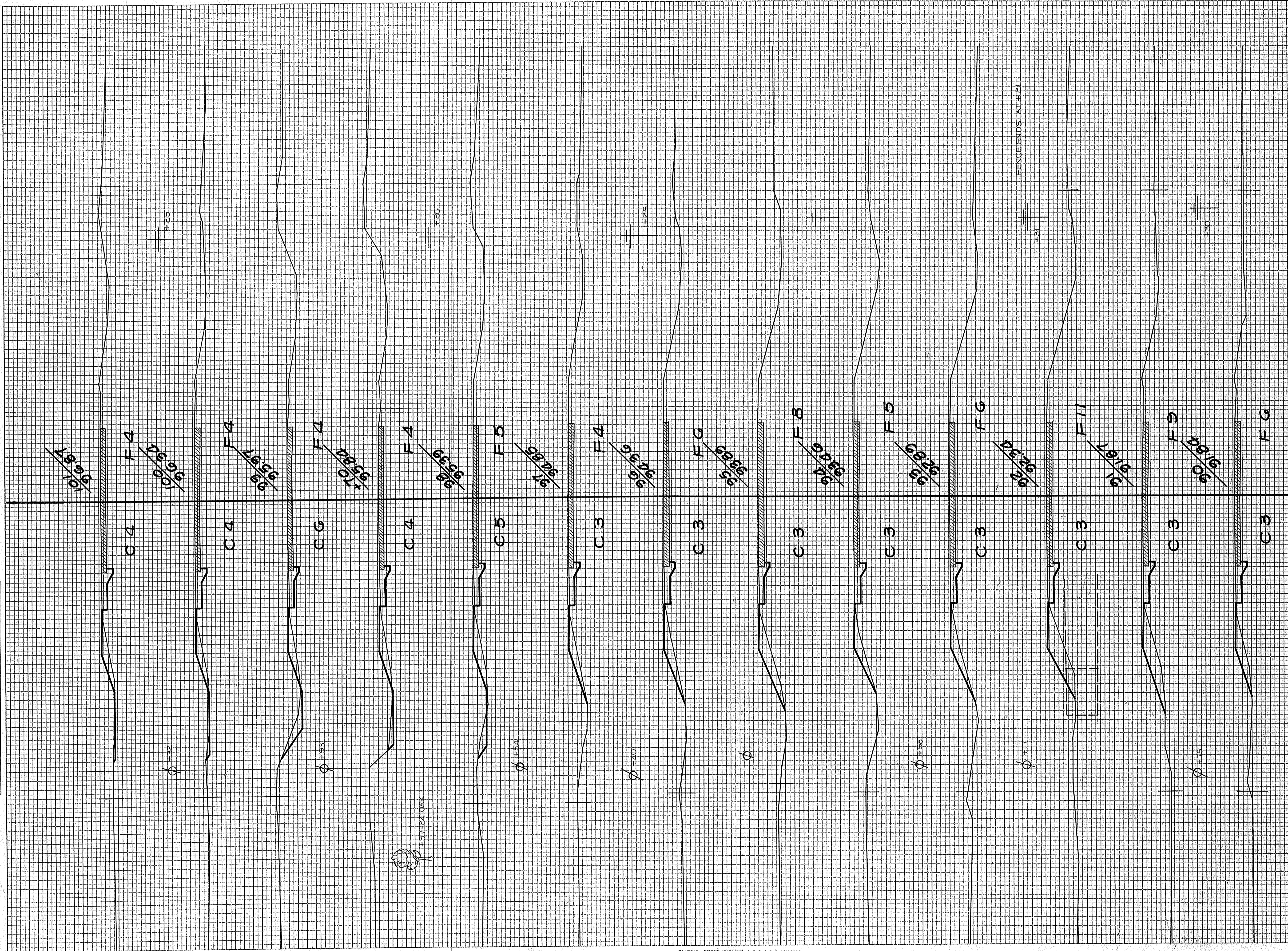






ORIGINAL SURVEY BY DATE  
 SURVEYED BY **B. J. EWING** 12/23/33  
 NOTE BOOK NO. **10608A** TEMPLATES **1-33**  
 AREAS CHECKED **1-33**

FINAL SURVEY BY DATE  
 SURVEYED BY \_\_\_\_\_ 12/23/33  
 NOTE BOOK NO. \_\_\_\_\_ TEMPLATES \_\_\_\_\_  
 AREAS CHECKED \_\_\_\_\_



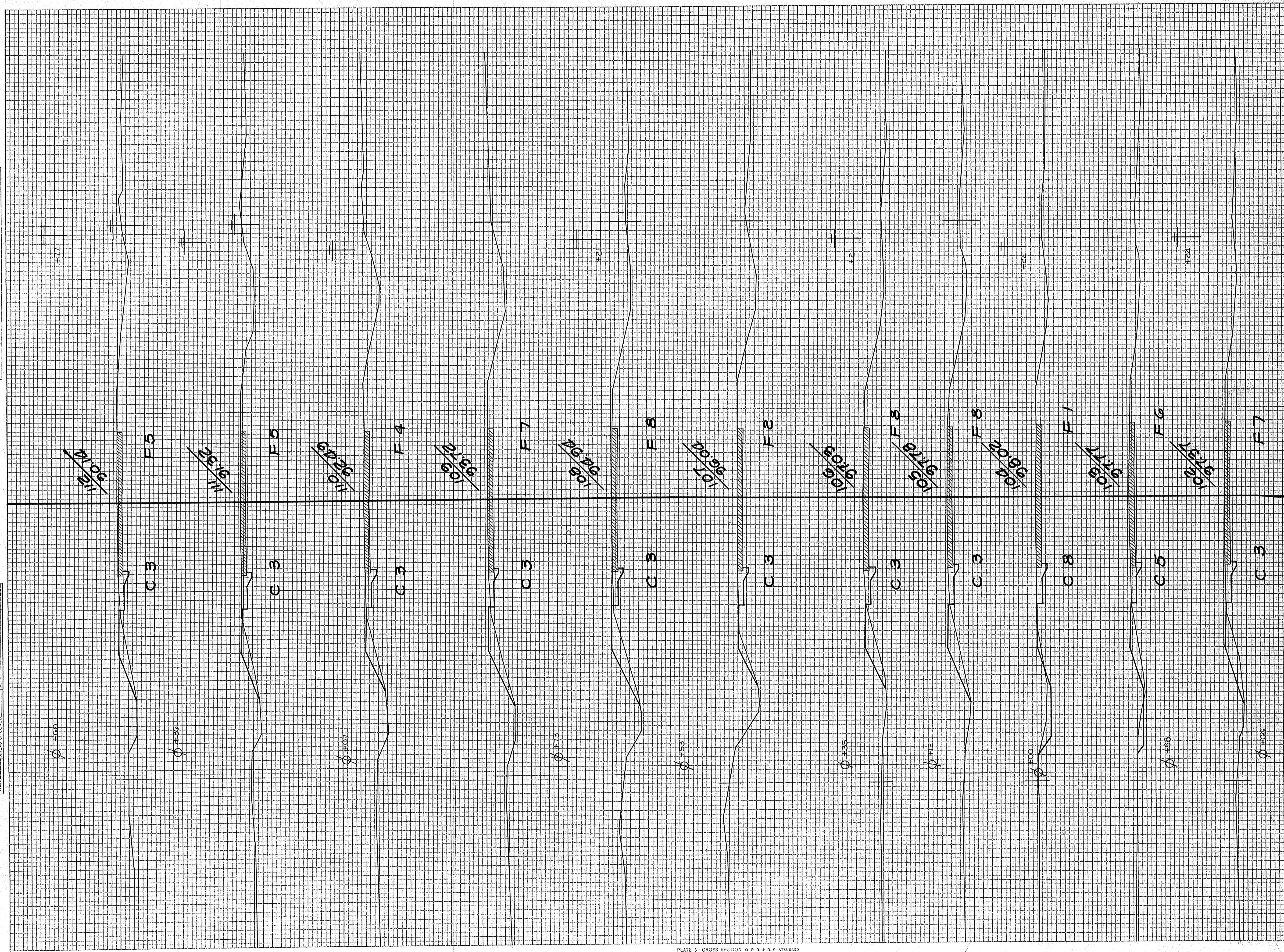
STATION	END AREA			DISTANCE			YARDAGE			TOTAL
	EARTH	ROCK	FILL	EARTH	ROCK	FILL	EARTH	ROCK	FILL	
101	4	15	4	15	15	4	15	15	4	15
100	4	19	4	11	15	4	11	15	4	17
99	4	6	4	6	11	4	6	11	4	2
98	4	4	4	4	5	4	4	5	4	11
97	3	3	3	3	3	3	3	3	3	15
96	3	3	3	3	3	3	3	3	3	24
95	3	3	3	3	3	3	3	3	3	20
94	3	3	3	3	3	3	3	3	3	32
93	3	3	3	3	3	3	3	3	3	37
92	3	3	3	3	3	3	3	3	3	28
91	3	3	3	3	3	3	3	3	3	15
90	3	3	3	3	3	3	3	3	3	15
89	3	3	3	3	3	3	3	3	3	15
<b>TOTAL</b>	<b>163</b>									

FED. ROAD DIST. NO. 4  
 STATE WISC.  
 DIVISION JOB NO. 3501  
 U.S.P.W. AID PROJ. NO. 353B  
 SHEET NO. 34  
 TOTAL SHEETS 56

FINAL SURVEYED BY DATE  
 SURVEY PLOTTED BY DATE  
 NOTE BOOK TEMPLATE NO. 10-33  
 AREAS CHECKED

ORIGINAL SURVEYED BY DATE  
 SURVEY PLOTTED BY DATE  
 NOTE BOOK TEMPLATE NO. 10-33  
 AREAS CHECKED

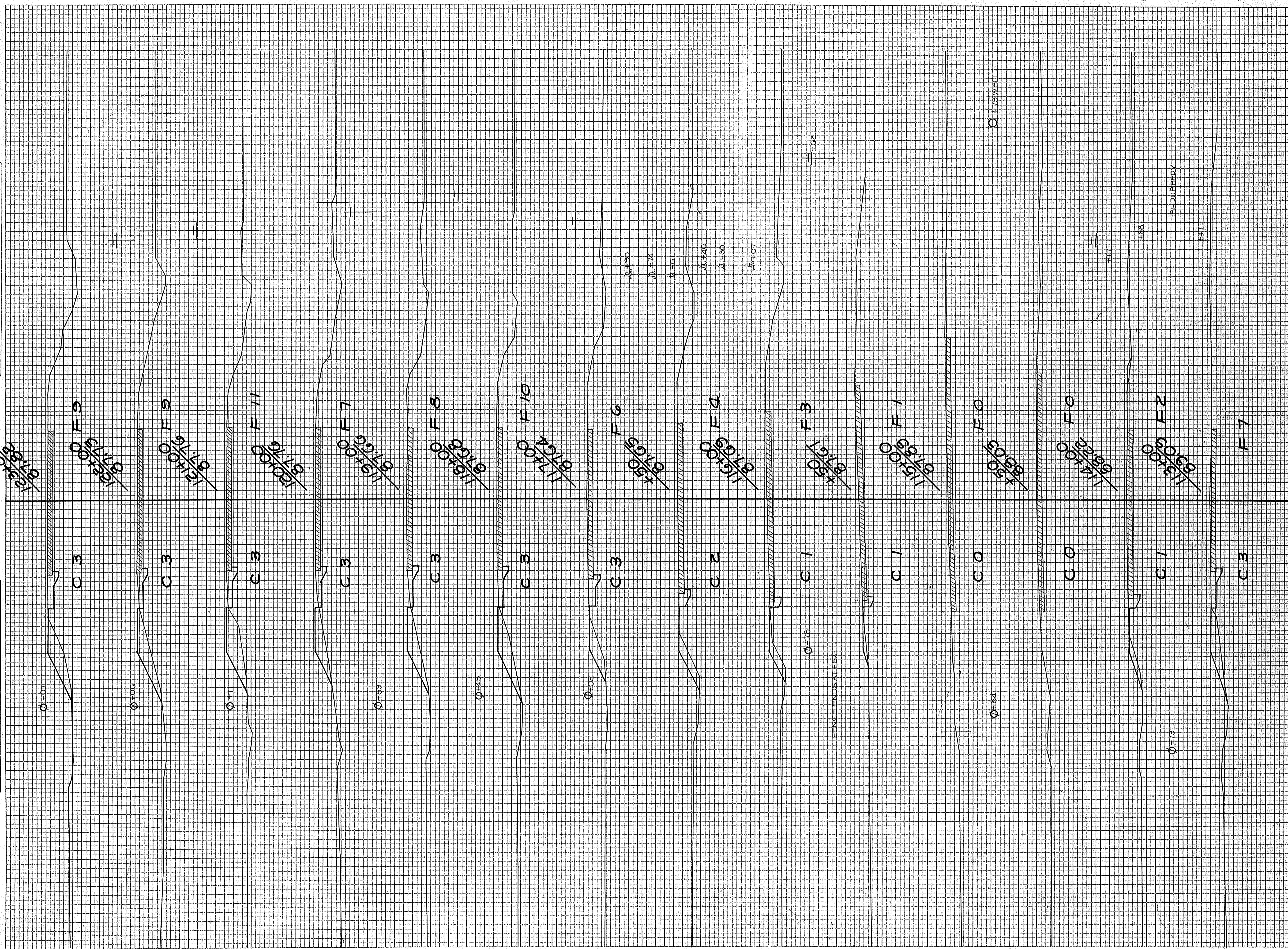
FED. ROAD DIST. NO. 4  
 STATE WISC. DIVISION  
 U.S.P.V. AID PROJ. NO. 353-B  
 SHEET NO. 35  
 TOTAL SHEETS 36  
 JOB NO. 3501



STATION	EARTH CUT		ROCK CUT		TOTAL		DISTANCE	YARDAGE	
	FT.	IN.	FT.	IN.	FT.	IN.		EARTH	TOTAL
101	4	0	0	0	4	0	4	13	13
102	3	0	0	0	3	0	3	15	15
103	5	0	0	0	5	0	5	24	24
104	8	0	0	0	8	0	8	24	24
105	3	0	0	0	3	0	3	20	20
106	3	0	0	0	3	0	3	11	11
TOTAL	30	0	0	0	30	0	30	203	203
66	11	11	11	11	11	11	11	11	11
12	19	17	20	28	18	19	126	104	104

FINAL SURVEYED BY DATE  
 SURVEY PLOTTED BY DATE  
 NOTE BOOK NO. 10-33  
 AREAS CHECKED

ORIGINAL SURVEYED BY DATE  
 SURVEY PLOTTED BY DATE  
 NOTE BOOK NO. 10-33  
 AREAS CHECKED



STATION	END AREA			DISTANCE			YARDAGE		
	EARTH	ROCK	FILL	EARTH	L. ROCK	S. ROCK	TOTAL	EARTH	TOTAL
123	3	0	0	11	0	0	0	11	0
122	3	0	0	11	0	0	0	11	0
121	3	0	0	11	0	0	0	11	0
120	3	0	0	7	0	0	0	7	0
119	3	0	0	8	0	0	0	8	0
118	3	0	0	10	0	0	0	10	0
117	3	0	0	6	0	0	0	6	0
116	2	0	0	2	0	0	0	2	0
115	0	0	0	0	0	0	0	0	0
114	1	0	0	2	0	0	0	2	0
113	3	0	0	7	0	0	0	7	0
112	3	0	0	5	0	0	0	5	0
<b>TOTAL</b>	<b>30</b>	<b>0</b>	<b>0</b>	<b>96</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>96</b>	<b>0</b>

FED. ROAD DIST. NO. 4  
 STATE WISC.  
 U.S.P.W. PROJ. NO. 353-B  
 SHEET NO. 36  
 TOTAL SHEETS 56  
 DIVISION JOB NO. 3501



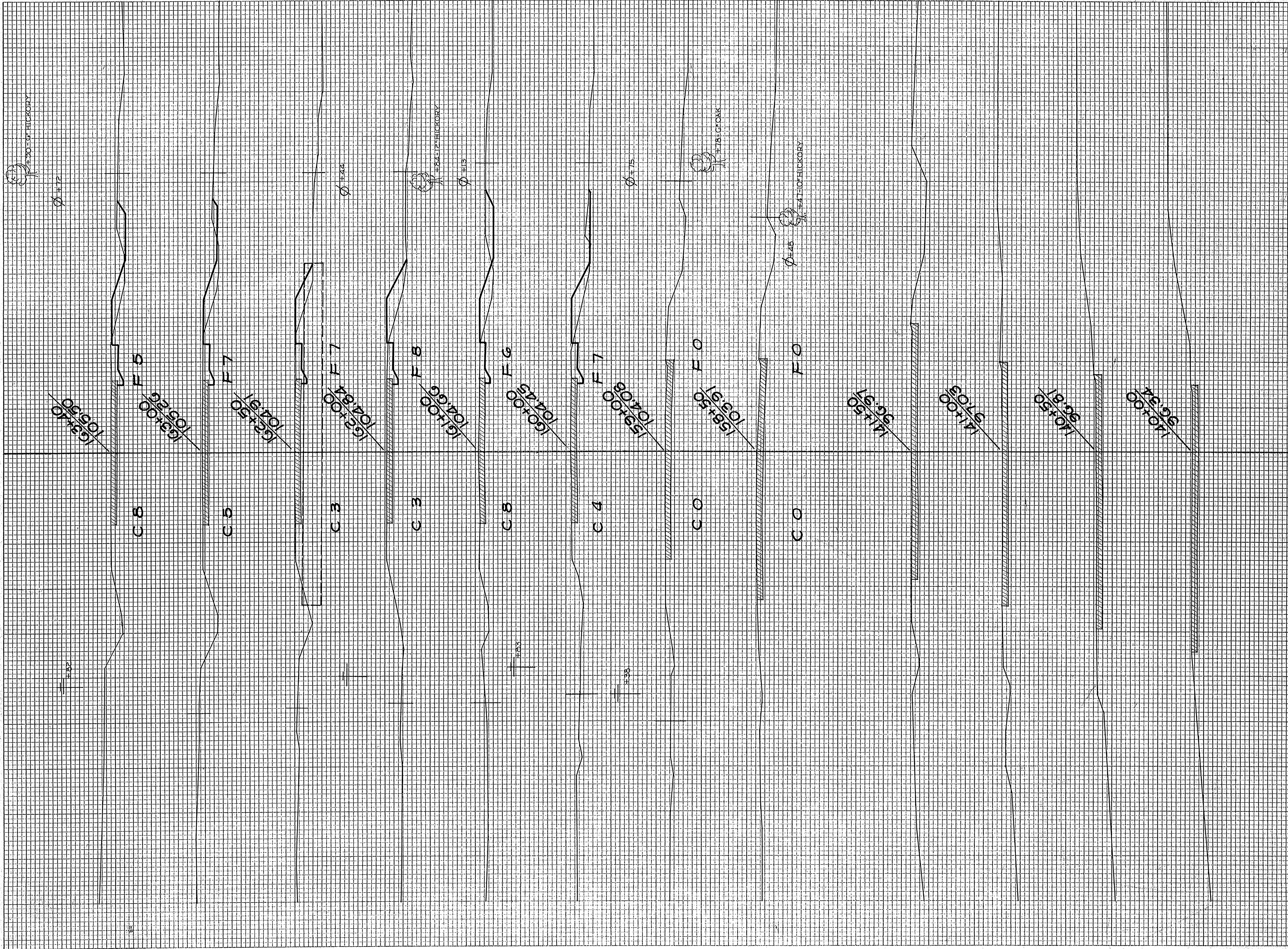




FINAL SURVEY PLOTTED  
 NO. 1000A  
 DATE 10-3-33

ORIGINAL SURVEY PLOTTED  
 NO. 1000A  
 DATE 10-3-33

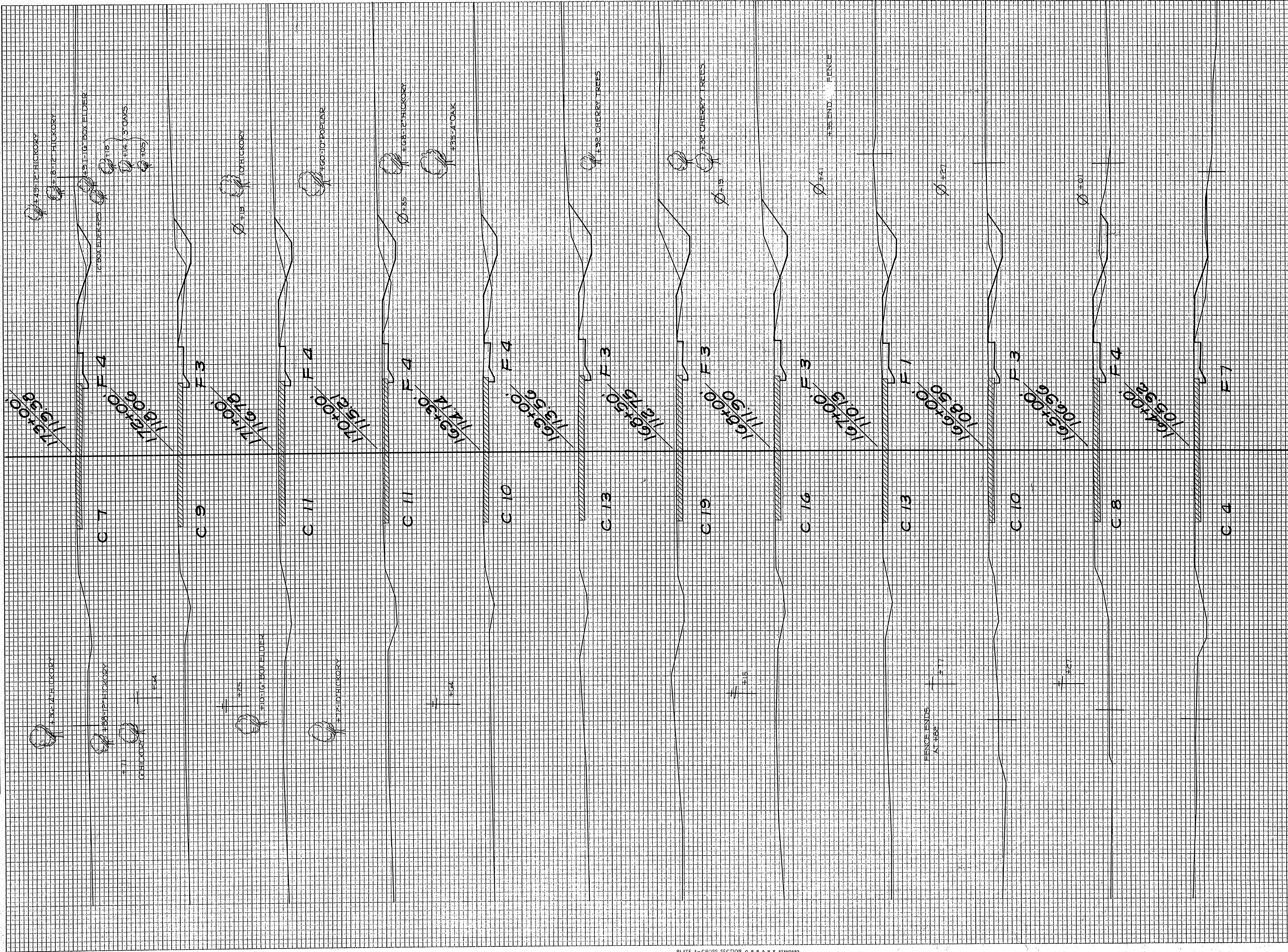
FED. ROAD DIST. NO. 4  
 STATE WISC.  
 U.S.P.V. PROJ. NO. 355 B.  
 SHEET NO. 39  
 TOTAL SHEETS 56  
 DIVISION JOB NO. 3501



STATION	END AREA			DISTANCE	YARDAGE			TOTAL	TOTAL	TOTAL
	EARTH	CUT	FILL		EARTH	L. ROCK	S. ROCK			
140	163	150	162	161	160	159				
5	5	3	3	8	4	0				
5	7	7	8	6	7	0				
9	7	6	20	22	7					
5	13	13	26	24	13					
TOTAL										

FINAL SURVEY PLOTTED BY DATE  
 SURVEY PLOTTED BY DATE  
 NOTE BOOK NO. 16024  
 AREA CHECKED

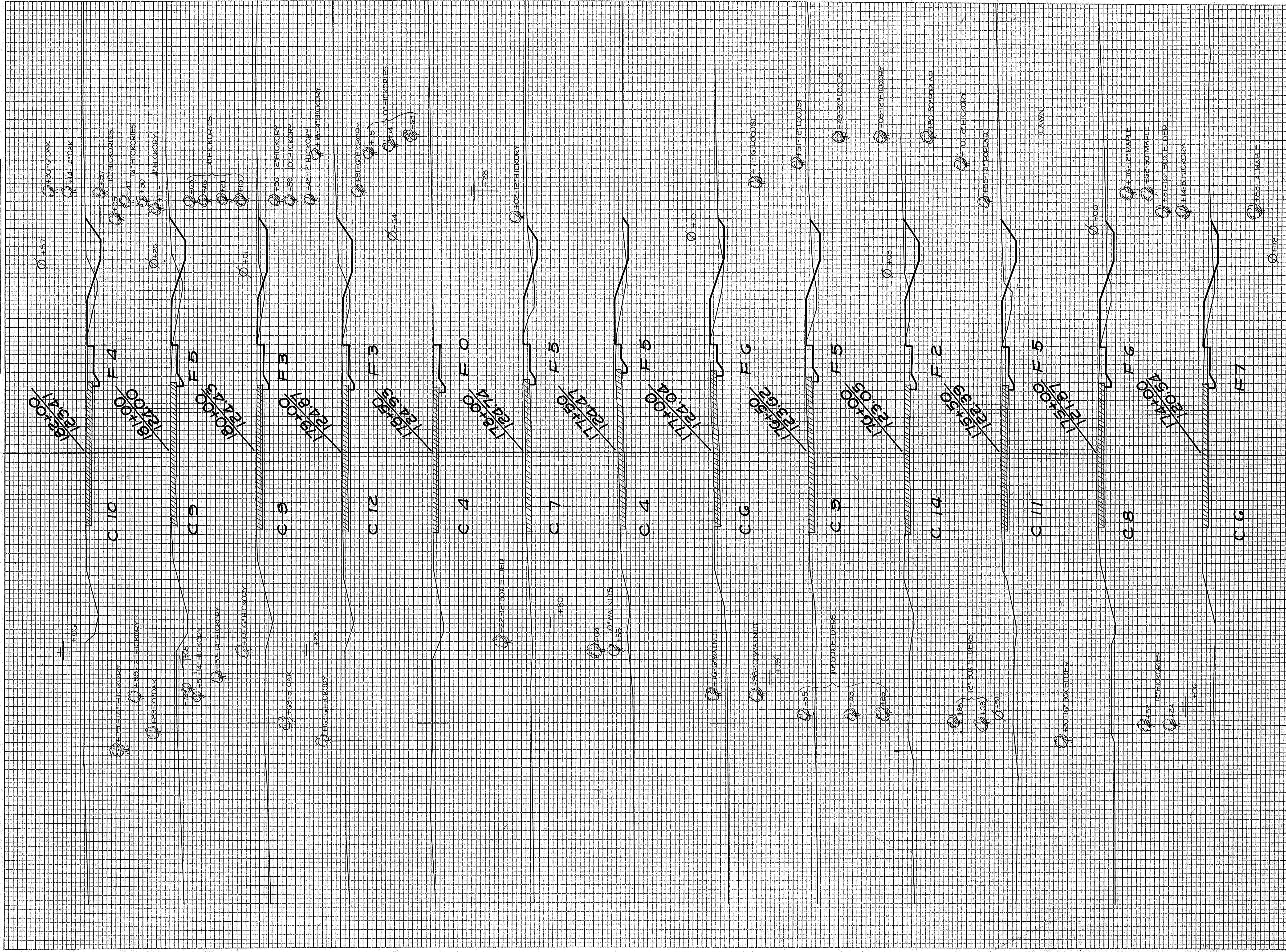
ORIGINAL SURVEY PLOTTED BY DATE  
 SURVEY PLOTTED BY DATE  
 NOTE BOOK NO. 16024  
 AREA CHECKED



STATION	CROSS AREA			DISTANCE			YARDAGE													
	ROCK	EARTH	FILL	ROCK	EARTH	FILL	ROCK	EARTH	FILL											
173	4	3	4	30	37	41	28	13	4	4	4	11	4	6	7	13	20	13	13	
172	3	3	4	31	37	41	28	13	4	4	4	3	3	3	3	3	3	3	3	3
171	4	4	4	32	38	42	29	14	5	5	5	4	4	4	4	4	4	4	4	4
170	4	4	4	33	39	43	30	15	6	6	6	5	5	5	5	5	5	5	5	5
169	4	4	4	34	40	44	31	16	7	7	7	6	6	6	6	6	6	6	6	6
168	4	4	4	35	41	45	32	17	8	8	8	7	7	7	7	7	7	7	7	7
167	4	4	4	36	42	46	33	18	9	9	9	8	8	8	8	8	8	8	8	8
166	4	4	4	37	43	47	34	19	10	10	10	9	9	9	9	9	9	9	9	9
165	4	4	4	38	44	48	35	20	11	11	11	10	10	10	10	10	10	10	10	10
164	4	4	4	39	45	49	36	21	12	12	12	11	11	11	11	11	11	11	11	11
163	4	4	4	40	46	50	37	22	13	13	13	12	12	12	12	12	12	12	12	12
162	4	4	4	41	47	51	38	23	14	14	14	13	13	13	13	13	13	13	13	13
161	4	4	4	42	48	52	39	24	15	15	15	14	14	14	14	14	14	14	14	14
160	4	4	4	43	49	53	40	25	16	16	16	15	15	15	15	15	15	15	15	15
159	4	4	4	44	50	54	41	26	17	17	17	16	16	16	16	16	16	16	16	16
158	4	4	4	45	51	55	42	27	18	18	18	17	17	17	17	17	17	17	17	17
157	4	4	4	46	52	56	43	28	19	19	19	18	18	18	18	18	18	18	18	18
156	4	4	4	47	53	57	44	29	20	20	20	19	19	19	19	19	19	19	19	19
155	4	4	4	48	54	58	45	30	21	21	21	20	20	20	20	20	20	20	20	20
154	4	4	4	49	55	59	46	31	22	22	22	21	21	21	21	21	21	21	21	21
153	4	4	4	50	56	60	47	32	23	23	23	22	22	22	22	22	22	22	22	22
152	4	4	4	51	57	61	48	33	24	24	24	23	23	23	23	23	23	23	23	23
151	4	4	4	52	58	62	49	34	25	25	25	24	24	24	24	24	24	24	24	24
150	4	4	4	53	59	63	50	35	26	26	26	25	25	25	25	25	25	25	25	25
149	4	4	4	54	60	64	51	36	27	27	27	26	26	26	26	26	26	26	26	26
148	4	4	4	55	61	65	52	37	28	28	28	27	27	27	27	27	27	27	27	27
147	4	4	4	56	62	66	53	38	29	29	29	28	28	28	28	28	28	28	28	28
146	4	4	4	57	63	67	54	39	30	30	30	29	29	29	29	29	29	29	29	29
145	4	4	4	58	64	68	55	40	31	31	31	30	30	30	30	30	30	30	30	30
144	4	4	4	59	65	69	56	41	32	32	32	31	31	31	31	31	31	31	31	31
143	4	4	4	60	66	70	57	42	33	33	33	32	32	32	32	32	32	32	32	32
142	4	4	4	61	67	71	58	43	34	34	34	33	33	33	33	33	33	33	33	33
141	4	4	4	62	68	72	59	44	35	35	35	34	34	34	34	34	34	34	34	34
140	4	4	4	63	69	73	60	45	36	36	36	35	35	35	35	35	35	35	35	35
139	4	4	4	64	70	74	61	46	37	37	37	36	36	36	36	36	36	36	36	36
138	4	4	4	65	71	75	62	47	38	38	38	37	37	37	37	37	37	37	37	37
137	4	4	4	66	72	76	63	48	39	39	39	38	38	38	38	38	38	38	38	38
136	4	4	4	67	73	77	64	49	40	40	40	39	39	39	39	39	39	39	39	39
135	4	4	4	68	74	78	65	50	41	41	41	40	40	40	40	40	40	40	40	40
134	4	4	4	69	75	79	66	51	42	42	42	41	41	41	41	41	41	41	41	41
133	4	4	4	70	76	80	67	52	43	43	43	42	42	42	42	42	42	42	42	42
132	4	4	4	71	77	81	68	53	44	44	44	43	43	43	43	43	43	43	43	43
131	4	4	4	72	78	82	69	54	45	45	45	44	44	44	44	44	44	44	44	44
130	4	4	4	73	79	83	70	55	46	46	46	45	45	45	45	45	45	45	45	45
129	4	4	4	74	80	84	71	56	47	47	47	46	46	46	46	46	46	46	46	46
128	4	4	4	75	81	85	72	57	48	48	48	47	47	47	47	47	47	47	47	47
127	4	4	4	76	82	86	73	58	49	49	49	48	48	48	48	48	48	48	48	48
126	4	4	4	77	83	87	74	59	50	50	50	49	49	49	49	49	49	49	49	49
125	4	4	4	78	84	88	75	60	51	51	51	50	50	50	50	50	50	50	50	50
124	4	4	4	79	85	89	76	61	52	52	52	51	51	51	51	51	51	51	51	51
123	4	4	4	80	86	90	77	62	53	53	53	52	52	52	52	52	52	52	52	52
122	4	4	4	81	87	91	78	63	54	54	54	53	53	53	53	53	53	53	53	53
121	4	4	4	82	88	92	79	64	55	55	55	54	54	54	54	54	54	54	54	54
120	4	4	4	83	89	93	80	65	56	56	56	55	55	55	55	55	55	55	55	55
119	4	4	4	84	90	94	81	66	57	57	57	56	56	56	56	56	56	56	56	56
118	4	4	4	85	91	95	82	67	58	58	58	57	57	57	57	57	57	57	57	57
117	4	4	4	86	92	96	83	68	59	59	59	58	58	58	58	58	58	58	58	58
116	4	4	4	87	93	97	84	69	60	60	60	59	59	59	59	59	59	59	59	59
115	4	4	4	88	94	98	85	70	61	61	61	60	60	60	60	60	60	60	60	60
114	4	4	4	89	95	99	86	71	62	62	62	61	61	61	61	61	61	61	61	61
113	4	4	4	90	96	100	87	72	63	63	63	62	62	62	62	62	62	62	62	62
112	4	4	4	91	97	101	88	73	64	64	64	63	63	63	63	63	63	63	63	63
111	4	4	4	92	98	102	89	74	65	65	65	64	64	64	64	64	64	64	64	64
110	4	4	4	93	99	103	90	75	66	66	66	65	65	65	65	65	65	65	65	65
109	4	4	4	94	100	104	91	76	67	67	67	66	66	66	66	66	66	66	66	66
108	4	4	4	95	101	105	92	77	68	68	68	67	67	67	67	67	67	67	67	67
107	4	4	4	96	102	106	93	78	69	69	69	68	68	68	68	68	68	68	68	68
106	4	4	4	97	103	107	94	79	70	70	70	69	69	69	69	69	69	69	69	69
105	4	4	4	98	104	108	95	80	71	71	71	70	70	70	70	70	70	70	70	70
104	4	4	4	99	105	109	96	81	72	72	72	71	71</							

ORIGINAL SURVEYED BY DATE  
 SURVEY P. J. S. 10-33  
 NOTE BOOK T. P. 11-33  
 NO. 2022A AREAS CHECKED 12-33

FINAL SURVEYED BY DATE  
 SURVEY P. J. S. 10-33  
 NOTE BOOK T. P. 11-33  
 NO. 2022A AREAS CHECKED 12-33



TOTAL	STATION										END AREA			YARDAGE											
	182	181	180	179	+50	178	+50	177	+50	176	+50	175	174	173	EARTH	ROCK	FILL	DISTANCE	EARTH	L. ROCK	S. ROCK	TOTAL	TOTAL	FILE	
10	9	9	12	4	7	4	6	6	5	2	5	6	5	4	7				24				17	15	11
4	5	3	3	0	5	5	6	6	5	2	5	6	5	4					24				17	15	11
35	33	39	15	9	11	18	13	22	22	19	26	24													
141																									

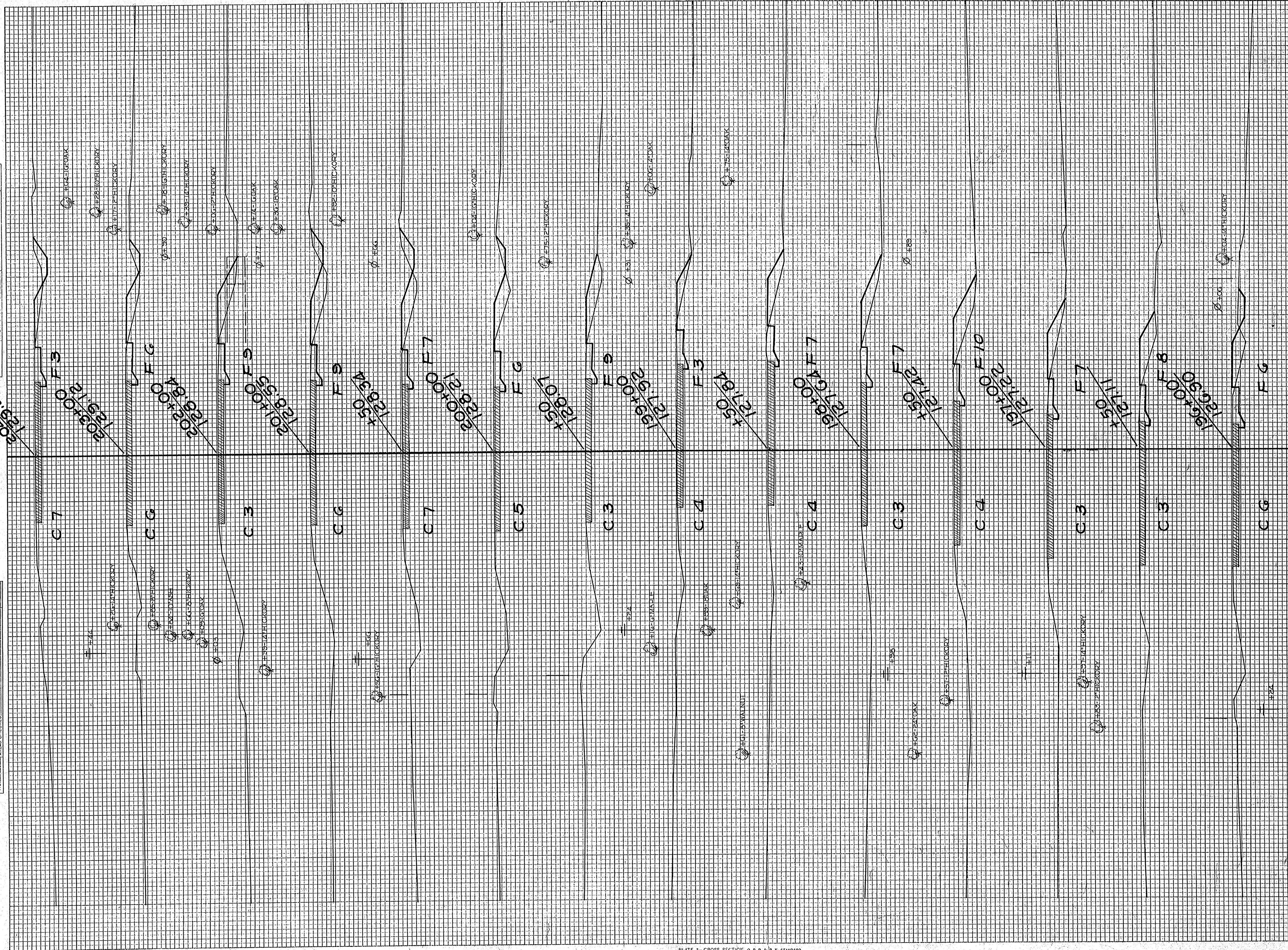
FED. ROAD DIST. NO. 4  
 STATE WISC.  
 U.S.P.V. PROJ. NO. 393-B  
 SHEET NO. 41  
 TOTAL SHEETS 56  
 DIVISION JOB NO. 350





FINAL SURVEY PLOTTED  
 NOTE BOOK AREAS CHECKED  
 NO. 10-33

ORIGINAL SURVEY PLOTTED  
 NOTE BOOK AREAS CHECKED  
 NO. 10-33



STATION	END AREA		YARDAGE	
	EARTH CUT	FILL	EARTH CUT	TOTAL FILL
204	7	3	17	15
203	6	3	17	15
202	3	3	11	11
201	6	7	13	11
200	5	6	11	15
199	3	4	7	11
198	4	3	7	15
197	3	3	7	17
196	4	3	7	13
195	3	3	7	13
156	6	4	10	13
155	10	8	15	13
154	4	4	7	13
153	3	3	7	13
152	3	3	7	13
151	3	3	7	13
150	3	3	7	13
TOTAL	17	28	33	15
	17	28	33	15

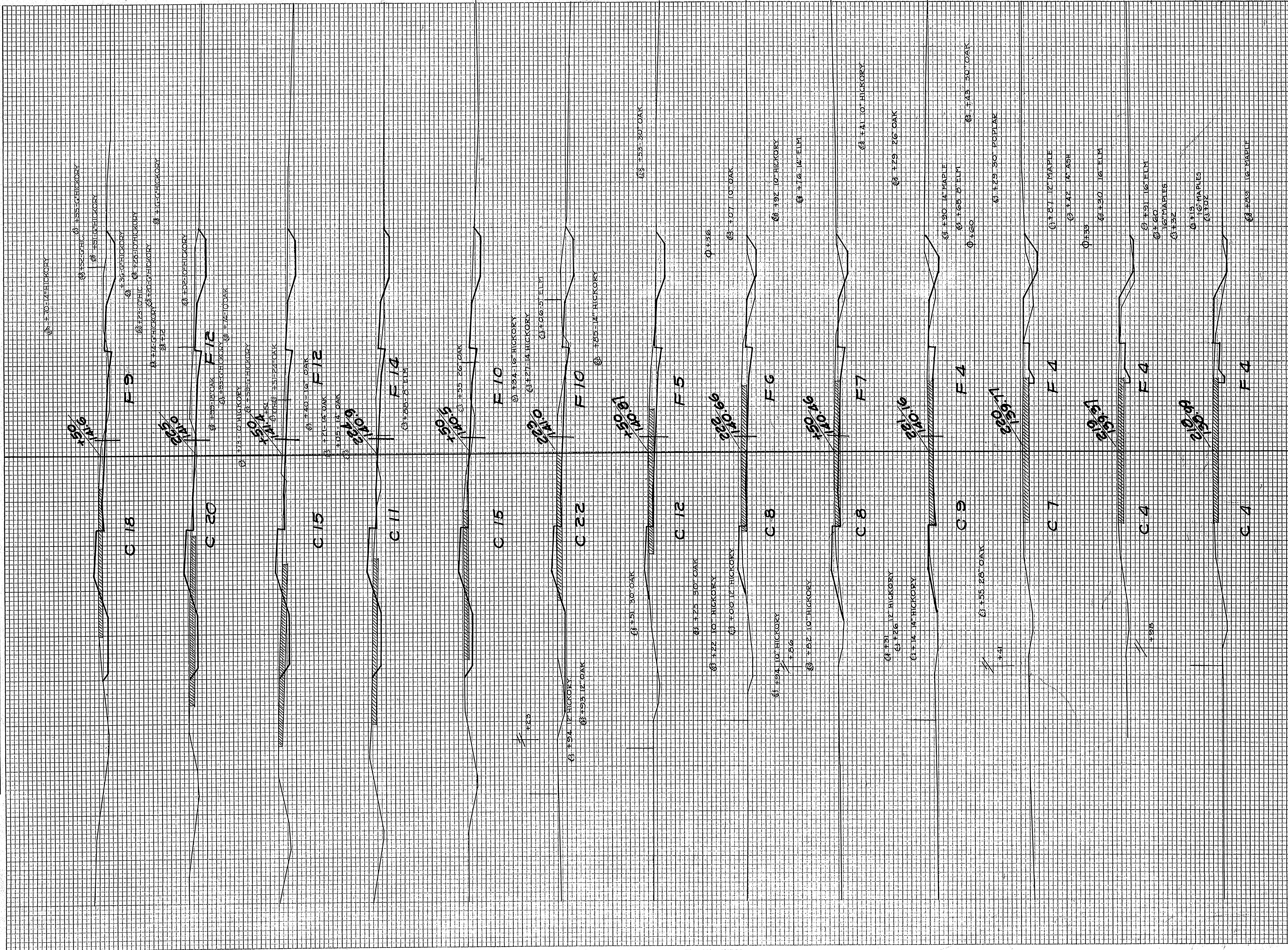
FED. ROAD DIST. NO. 4  
 STATE WISC.  
 DIVISION JOB NO. 350

U.S.P.V. AID PROJ. NO. 333B  
 SHEET NO. 44  
 TOTAL SHEETS 56



ORIGINAL SURVEYED BY DATE  
 SURVEY PLOTTED BY  
 NOTE BOOK TEMPLATE NO. 7611A AREAS CHECKED

FINAL SURVEYED BY DATE  
 SURVEY PLOTTED BY  
 NOTE BOOK TEMPLATE NO. 7611A AREAS CHECKED



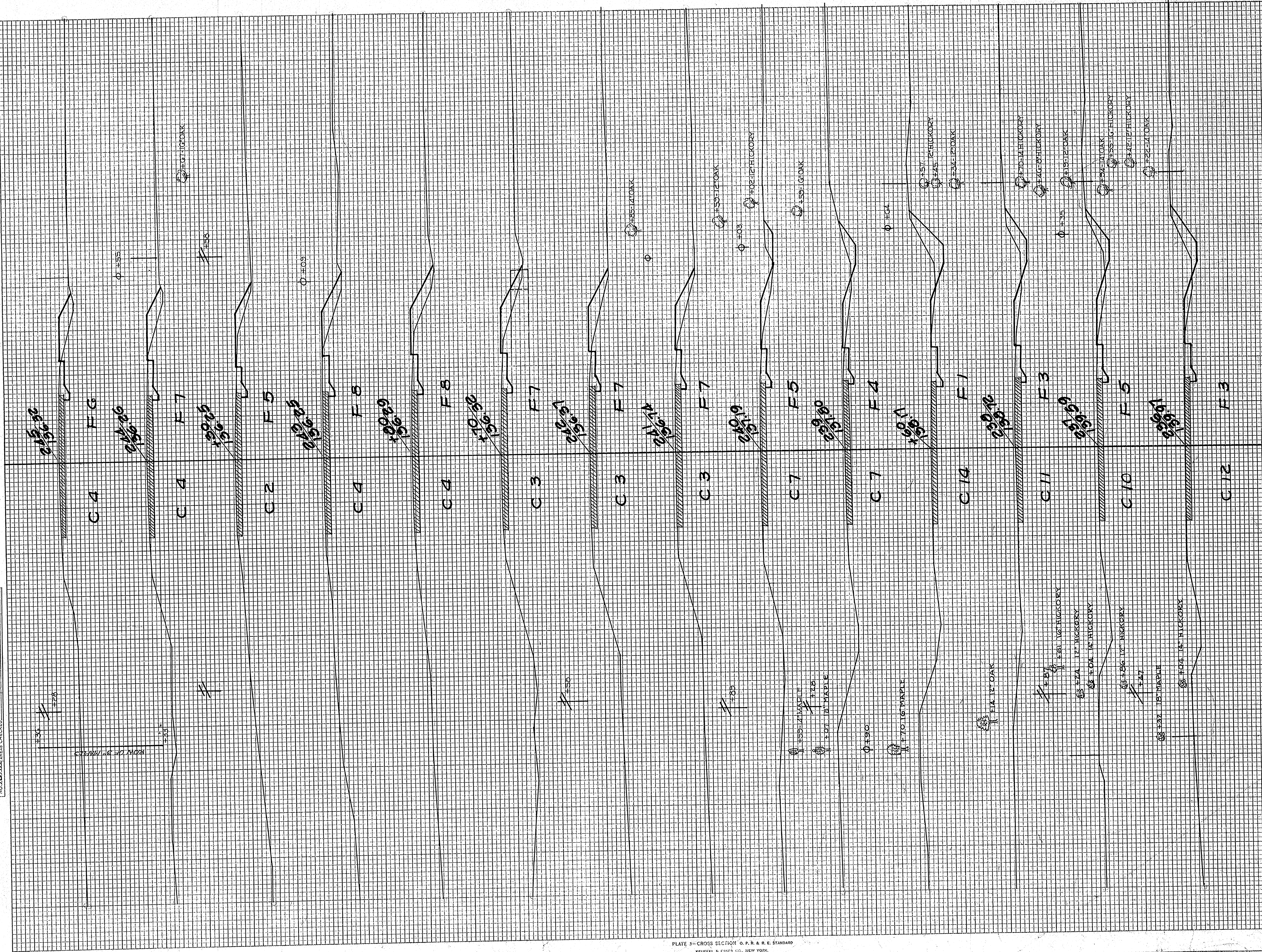
TOTAL	STATION																	END AREA		VARDAGE																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																				
	150	225	150	224	150	223	150	222	150	221	150	220	219	218	217	216	215	214	213	212	211	210	209	208	207	206	205	204	203	202	201	200	199	198	197	196	195	194	193	192	191	190	189	188	187	186	185	184	183	182	181	180	179	178	177	176	175	174	173	172	171	170	169	168	167	166	165	164	163	162	161	160	159	158	157	156	155	154	153	152	151	150	149	148	147	146	145	144	143	142	141	140	139	138	137	136	135	134	133	132	131	130	129	128	127	126	125	124	123	122	121	120	119	118	117	116	115	114	113	112	111	110	109	108	107	106	105	104	103	102	101	100	99	98	97	96	95	94	93	92	91	90	89	88	87	86	85	84	83	82	81	80	79	78	77	76	75	74	73	72	71	70	69	68	67	66	65	64	63	62	61	60	59	58	57	56	55	54	53	52	51	50	49	48	47	46	45	44	43	42	41	40	39	38	37	36	35	34	33	32	31	30	29	28	27	26	25	24	23	22	21	20	19	18	17	16	15	14	13	12	11	10	9	8	7	6	5	4	3	2	1	0	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100	101	102	103	104	105	106	107	108	109	110	111	112	113	114	115	116	117	118	119	120	121	122	123	124	125	126	127	128	129	130	131	132	133	134	135	136	137	138	139	140	141	142	143	144	145	146	147	148	149	150	151	152	153	154	155	156	157	158	159	160	161	162	163	164	165	166	167	168	169	170	171	172	173	174	175	176	177	178	179	180	181	182	183	184	185	186	187	188	189	190	191	192	193	194	195	196	197	198	199	200	201	202	203	204	205	206	207	208	209	210	211	212	213	214	215	216	217	218	219	220	221	222	223	224	225	226	227	228	229	230	231	232	233	234	235	236	237	238	239	240	241	242	243	244	245	246	247	248	249	250	251	252	253	254	255	256	257	258	259	260	261	262	263	264	265	266	267	268	269	270	271	272	273	274	275	276	277	278	279	280	281	282	283	284	285	286	287	288	289	290	291	292	293	294	295	296	297	298	299	300	301	302	303	304	305	306	307	308	309	310	311	312	313	314	315	316	317	318	319	320	321	322	323	324	325	326	327	328	329	330	331	332	333	334	335	336	337	338	339	340	341	342	343	344	345	346	347	348	349	350	351	352	353	354	355	356	357	358	359	360	361	362	363	364	365	366	367	368	369	370	371	372	373	374	375	376	377	378	379	380	381	382	383	384	385	386	387	388	389	390	391	392	393	394	395	396	397	398	399	400	401	402	403	404	405	406	407	408	409	410	411	412	413	414	415	416	417	418	419	420	421	422	423	424	425	426	427	428	429	430	431	432	433	434	435	436	437	438	439	440	441	442	443	444	445	446	447	448	449	450	451	452	453	454	455	456	457	458	459	460	461	462	463	464	465	466	467	468	469	470	471	472	473	474	475	476	477	478	479	480	481	482	483	484	485	486	487	488	489	490	491	492	493	494	495	496	497	498	499	500	501	502	503	504	505	506	507	508	509	510	511	512	513	514	515	516	517	518	519	520	521	522	523	524	525	526	527	528	529	530	531	532	533	534	535	536	537	538	539	540	541	542	543	544	545	546	547	548	549	550	551	552	553	554	555	556	557	558	559	560	561	562	563	564	565	566	567	568	569	570	571	572	573	574	575	576	577	578	579	580	581	582	583	584	585	586	587	588	589	590	591	592	593	594	595	596	597	598	599	600	601	602	603	604	605	606	607	608	609	610	611	612	613	614	615	616	617	618	619	620	621	622	623	624	625	626	627	628	629	630	631	632	633	634	635	636	637	638	639	640	641	642	643	644	645	646	647	648	649	650	651	652	653	654	655	656	657	658	659	660	661	662	663	664	665	666	667	668	669	670	671	672	673	674	675	676	677	678	679	680	681	682	683	684	685	686	687	688	689	690	691	692	693	694	695	696	697	698	699	700	701	702	703	704	705	706	707	708	709	710	711	712	713	714	715	716	717	718	719	720	721	722	723	724	725	726	727	728	729	730	731	732	733	734	735	736	737	738	739	740	741	742	743	744	745	746	747	748	749	750	751	752	753	754	755	756	757	758	759	760	761	762	763	764	765	766	767	768	769	770	771	772	773	774	775	776	777	778	779	780	781	782	783	784	785	786	787	788	789	790	791	792	793	794	795	796	797	798	799	800	801	802	803	804	805	806	807	808	809	810	811	812	813	814	815	816	817	818	819	820	821	822	823	824	825	826	827	828	829	830	831	832	833	834	835	836	837	838	839	840	841	842	843	844	845	846	847	848	849	850	851	852	853	854	855	856	857	858	859	860	861	862	863	864	865	866	867	868	869	870	871	872	873	874	875	876	877	878	879	880	881	882	883	884	885	886	887	888	889	890	891	892	893	894	895	896	897	898	899	900	901	902	903	904	905	906	907	908	909	910	911	912	913	914	915	916	917	918	919	920	921	922	923	924	925	926	927	928	929	930	931	932	933	934	935	936	937	938	939	940	941	942	943	944	945	946	947	948	949	950	951	952	953	954	955	956	957	958	959	960	961	962	963	964	965	966	967	968	969	970	971	972	973	974	975	976	977	978	979	980	981	982	983	984	985	986	987	988	989	990	991	992	993	994	995	996	997	998	999	1000	1001	1002	1003	1004	1005	1006	1007	1008	1009	1010	1011	1012	1013	1014	1015	1016	1017	1018	1019	1020	1021	1022	1023	1024	1025	1026	1027	1028	1029	1030	1031	1032	1033	1034	1035	1036	1037	1038	1039	1040	1041	1042	1043	1044	1045	1046	1047	1048	1049	1050	1051	1052	1053	1054	1055	1056	1057	1058	1059	1060	1061	1062	1063	1064	1065	1066	1067	1068	1069	1070	1071	1072	1073	1074	1075	1076	1077	1078	1079	1080	1081	1082	1083	1084	1085	1086	1087	1088	1089	1090	1091	1092	1093	1094	1095	1096	1097	1098	1099	1100	1101	1102	1103	1104	1105	1106	1107	1108	1109	1110	1111	1112	1113	1114	1115	1116	1117	1118	1119	1120	1121	1122	1123	1124	1125	1126	1127	1128	1129	1130	1131	1132	1133	1134	1135	1136	1137	1138	1139	1140	1141	1142	1143	1144	1145	1146	1147	1148	1149	1150	1151	1152	1153	1154	1155	1156	1157	1158	1159	1160	1161	1162	1163	1164	1165	1166	1167	1168	1169	1170	1171	1172	1173	1174	1175	1176	1177	1178	1179	1180	1181	1182	1183	1184	1185	1186	1187	1188	1189	1190	1191	1192	1193	1194	1195	1196	1197	1198	1199	1200	1201	1202	1203	1204	1205	1206	1207	1208	1209	1210	1211	1212	1213	1214	1215	1216	1217	1218	1219	1220	1221	1222	1223	1224	1225	1226	1227	1228	1229	1230	1231	1232	1233	1234	1235	1236	1237	1238	1239	1240	1241	1242	1243	1244	1245	1246	1247	1248	1249	1250	1251	1252	1253	1254	1255	1256	1257	1258	1259	1260	1261	1262	1263	1264





FINAL SURVEY SURVEYED BY DATE  
 SURVEY PLOTTED BY  
 NOTE BOOK NO. AREAS CHECKED

ORIGINAL SURVEY PLOTTED BY DATE  
 SURVEY PLOTTED BY 10-31  
 NOTE BOOK NO. AREAS CHECKED 12-93



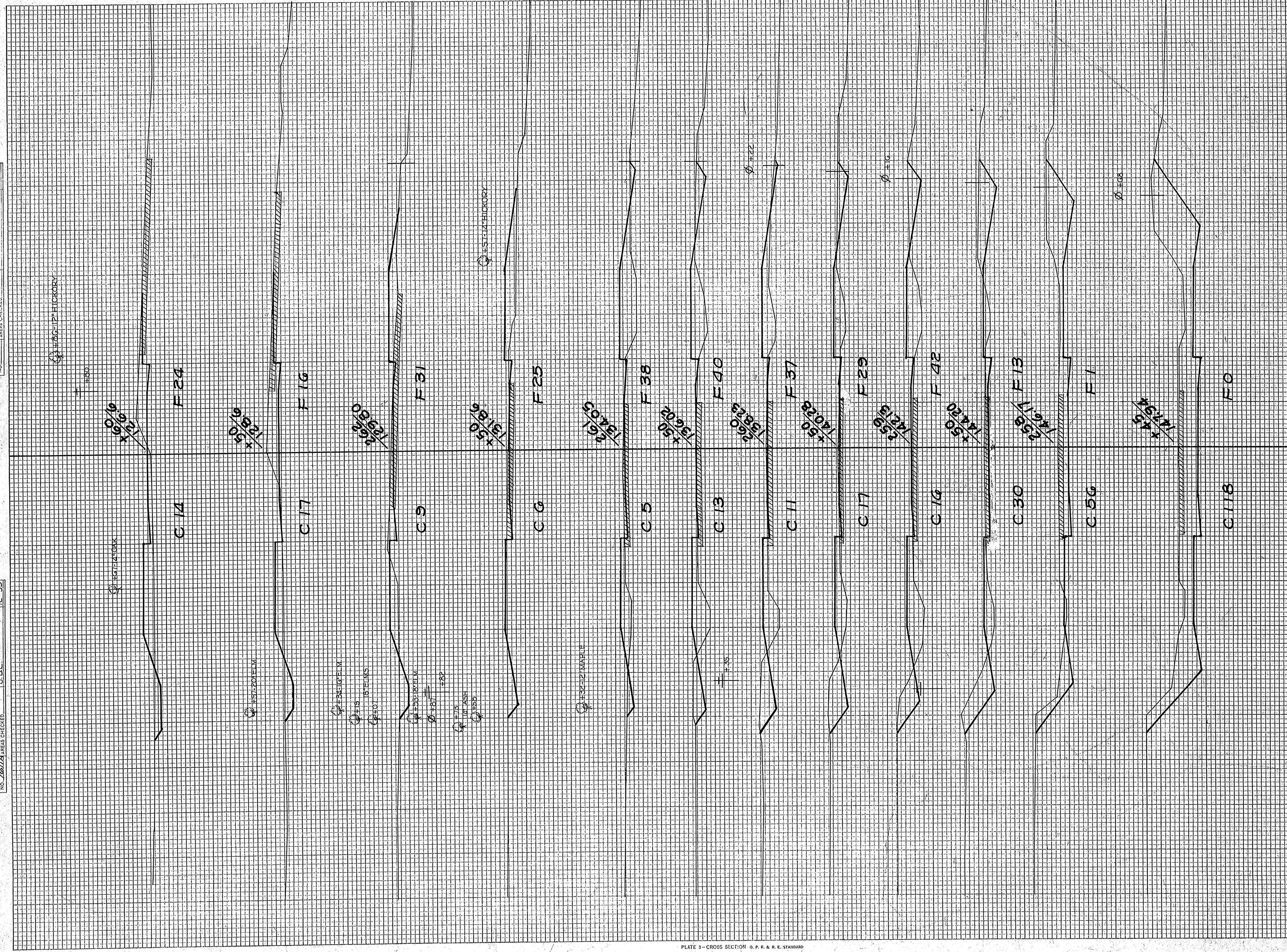
STATION	END AREA		YARDAGE				
	EARTH CUT	ROCK CUT	DISTANCE	EARTH CUT	L. ROCK	S. ROCK	TOTAL
235							
236							
237							
238							
239							
240							
241							
242							
243							
244							
245							
246							
247							
248							
249							
250							
251							
252							
253							
254							
255							
256							
257							
258							
259							
260							
261							
262							
263							
264							
265							
266							
267							
268							
269							
270							
271							
272							
273							
274							
275							
276							
277							
278							
279							
280							
281							
282							
283							
284							
285							
286							
287							
288							
289							
290							
291							
292							
293							
294							
295							
296							
297							



FINAL SURVEYED BY DATE  
 SURVEY P. J. J. M. W. F. 11-30-37  
 NOTE BOOK NO. 701/1 AREAS CHECKED U.S.C. 1-37  
 AREAS CHECKED U.S.C. 1-37

ORIGINAL SURVEYED BY DATE  
 SURVEY P. J. J. M. W. F. 11-30-37  
 NOTE BOOK NO. 701/1 AREAS CHECKED U.S.C. 1-37  
 AREAS CHECKED U.S.C. 1-37

FED. ROAD DIST. NO. 4 STATE WISC. DIVISION JOB NO. 3501  
 U.S.P.W. PROJ. NO. 3501 SHEET NO. 50 TOTAL SHEETS 56



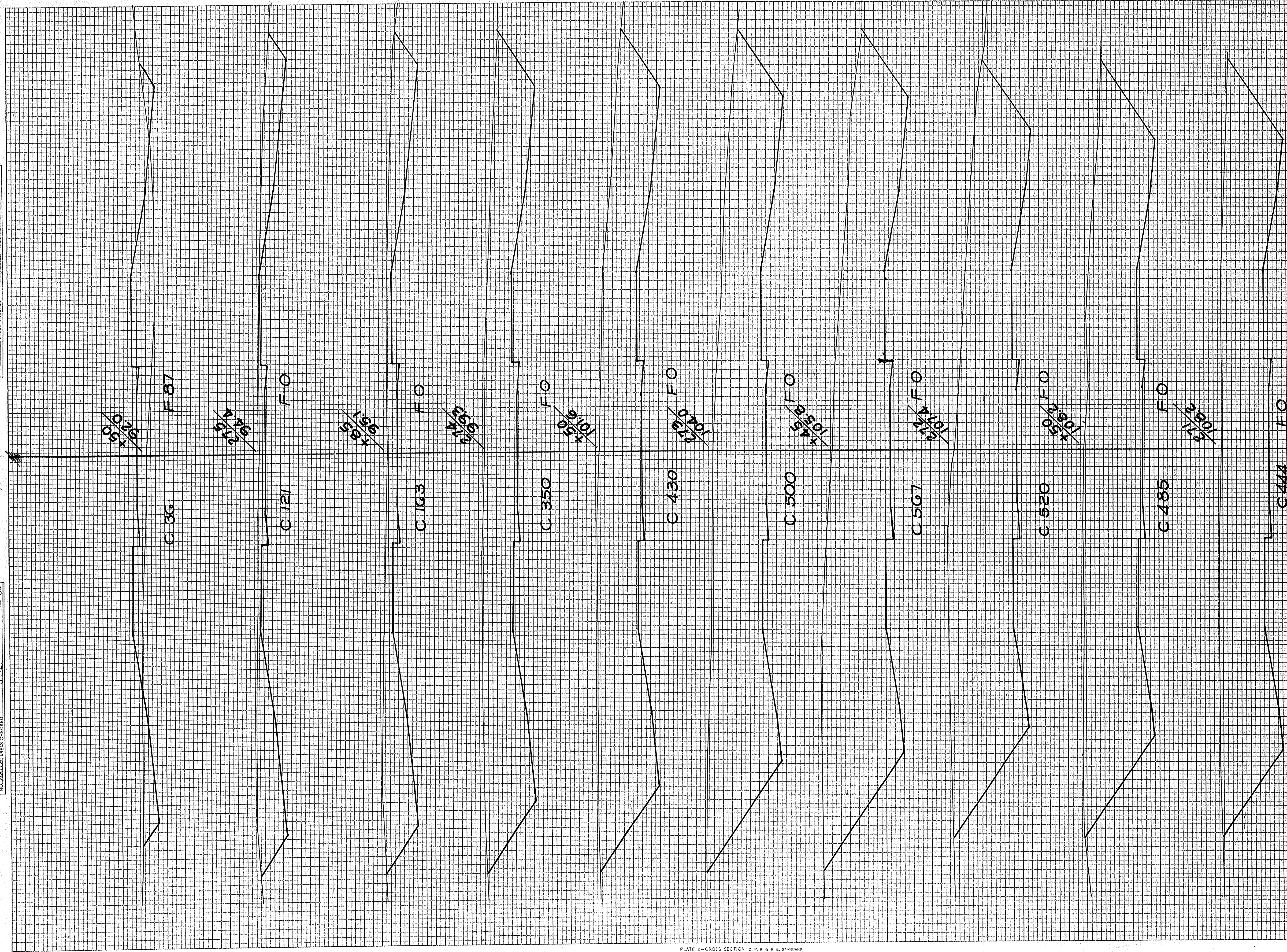
STATION	END AREA			DISTANCE	YARDAGE							
	EARTH CUT	ROCK CUT	FILL		EARTH CUT	ROCK CUT	TOTAL					
150	262	150	261	11	17	22	26	32	43	80	176	167
14	17	9	6	5	13	11	17	16	30	56	118	82
24	16	31	25	38	40	37	29	42	13	1	0	0
G/19	G	24	13	11	17	22	26	32	43	80	176	167
497	7	44	52	59	72	70	61	67	50	13	2	0
TOTAL												



FINAL SURVEYED BY DATE  
 SURVEY PLOTTED BY  
 NOTE BOOK TEMPLATE NO.  
 AREAS CHECKED

ORIGINAL SURVEYED BY DATE  
 SURVEY PLOTTED BY  
 NOTE BOOK TEMPLATE NO.  
 AREAS CHECKED

U.S.P.V.  
 FED. ROAD DIST. NO. 4  
 STATE WISC.  
 PROJ. NO. 3385  
 SHEET NO. 52  
 TOTAL SHEETS 56  
 DIVISION JOB. NO. 5501

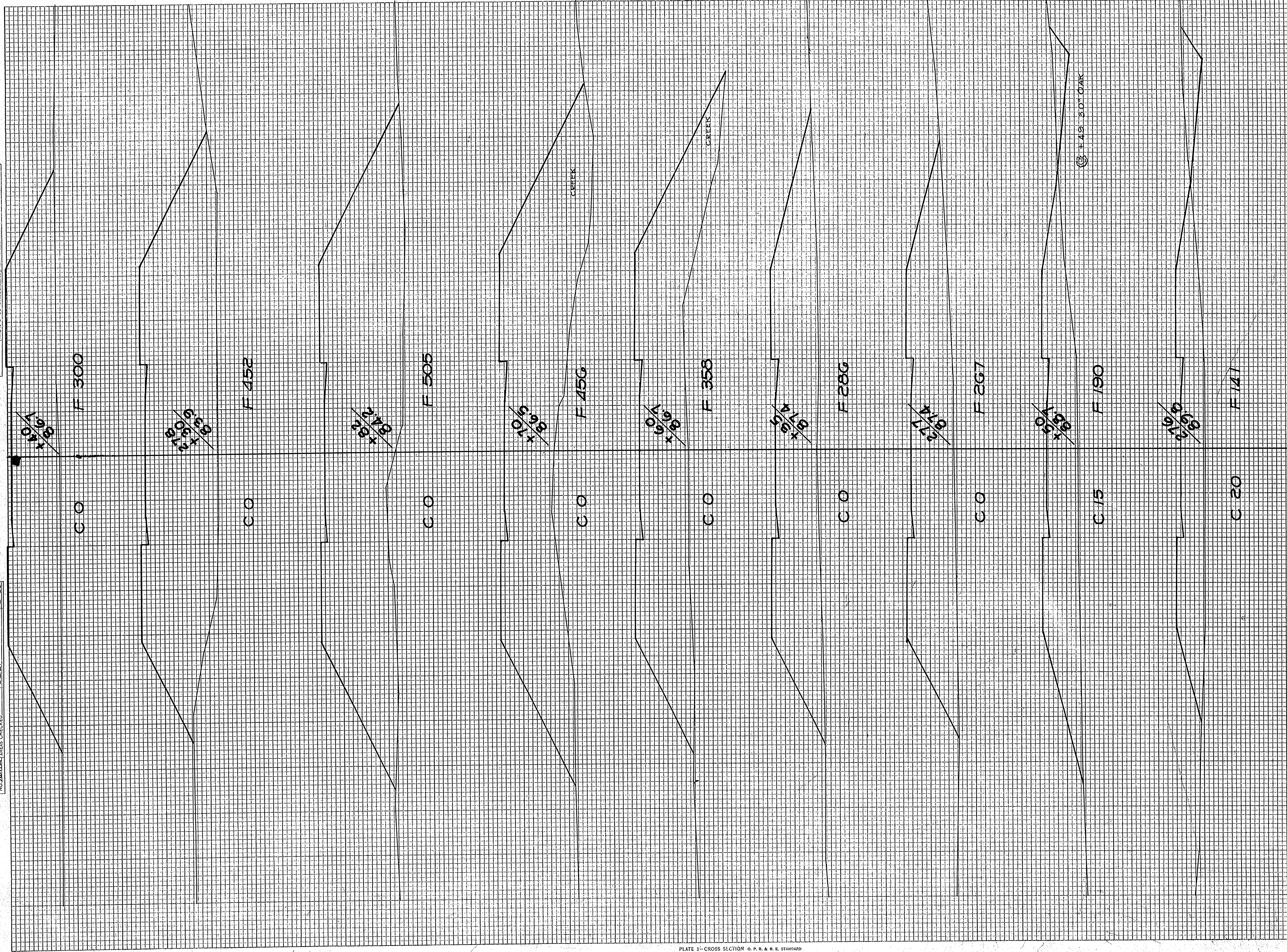


STATION	END AREA			YARDAGE		
	EARTH CUT	ROCK CUT	FILL	EARTH	ROCK	TOTAL
144	80	0	0	1087	306	331
274	163	0	0	1087	306	331
350	430	0	0	1087	306	331
485	567	0	0	1087	306	331
520	520	0	0	1087	306	331
567	444	0	0	1087	306	331
682	303	0	0	1087	306	331
<b>TOTAL</b>	<b>144</b>	<b>80</b>	<b>0</b>	<b>1087</b>	<b>306</b>	<b>331</b>
<b>7780</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>82</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

FINAL SURVEYED...  
 SURVEY PLOTTED...  
 NOTE BOOK...  
 NO. 7611A

ORIGINAL SURVEY...  
 SURVEY PLOTTED...  
 NOTE BOOK...  
 NO. 7611A

FED. ROAD DIST. NO. 4  
 STATE WISC.  
 DIVISION JOB NO. 3501  
 U.S.P.N. AID PROJ. NO. 3338  
 SHEET NO. 53  
 TOTAL SHEETS 50



STATION	END AREA			DISTANCE			YARDAGE		
	EARTH CUT	ROCK CUT	FILL	EARTH	L. ROCK	S. ROCK	EARTH	L. ROCK	S. ROCK
140	276	482	170	0	0	0	0	0	0
150	460	435	277	150	20	36	13	33	52
160	452	505	456	358	286	267	190	141	67
170	452	505	456	358	286	267	190	141	67
180	452	505	456	358	286	267	190	141	67
190	452	505	456	358	286	267	190	141	67
200	452	505	456	358	286	267	190	141	67
210	452	505	456	358	286	267	190	141	67
220	452	505	456	358	286	267	190	141	67
230	452	505	456	358	286	267	190	141	67
240	452	505	456	358	286	267	190	141	67
250	452	505	456	358	286	267	190	141	67
260	452	505	456	358	286	267	190	141	67
270	452	505	456	358	286	267	190	141	67
280	452	505	456	358	286	267	190	141	67
290	452	505	456	358	286	267	190	141	67
300	452	505	456	358	286	267	190	141	67
TOTAL	557	315	213	150	359	422	306	211	

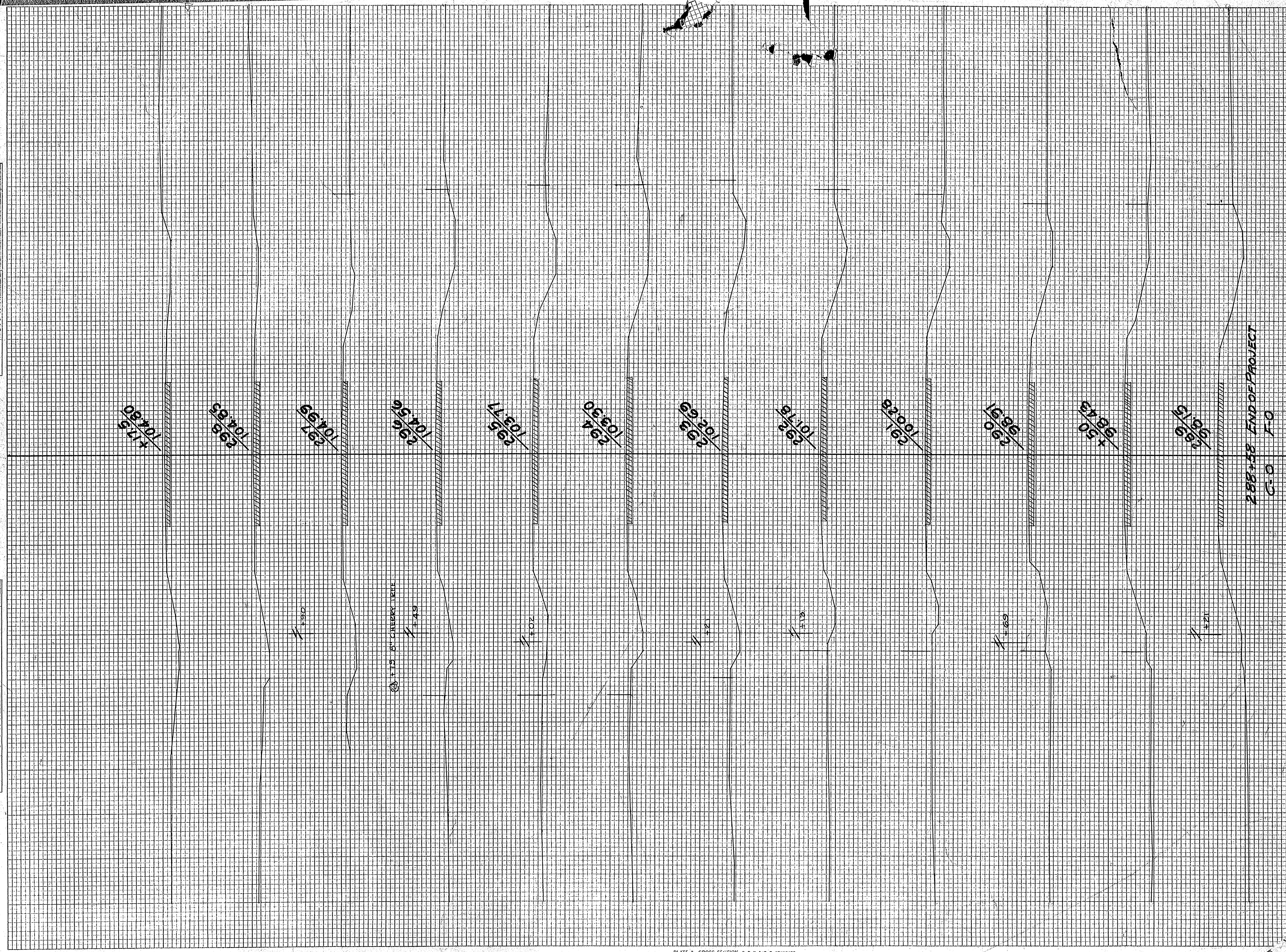






ORIGINAL SURVEYED BY DATE  
 PLOTTED BY 10-33  
 NOTE BOOK NO. 7614  
 AREA CHECKED

FINAL SURVEYED BY DATE  
 PLOTTED BY  
 NOTE BOOK NO.  
 AREA CHECKED



STATION	END AREA			DISTANCE	YARDAGE		
	EARTH CUT	ROCK	FILL		EARTH	L-ROCK	S-ROCK
+58	0		0	7			
288	7		52				
<b>SUB TOTAL</b>				7			
<b>TOTAL MILE %</b>				15726	9937		
<b>GRAND TOTAL U.S. PROJ. PROJECT #393-B</b>				15726	9937		
<b>TOTAL</b>				15726	9937		

288+58 END OF PROJECT  
 G-O F-O

FED. ROAD DIST. NO. 4  
 STATE WISC.  
 DIVISION JOB. NO. 3501  
 U.S.P.W. AID PROJ. NO. 383-B  
 SHEET NO. 56  
 TOTAL SHEETS 56